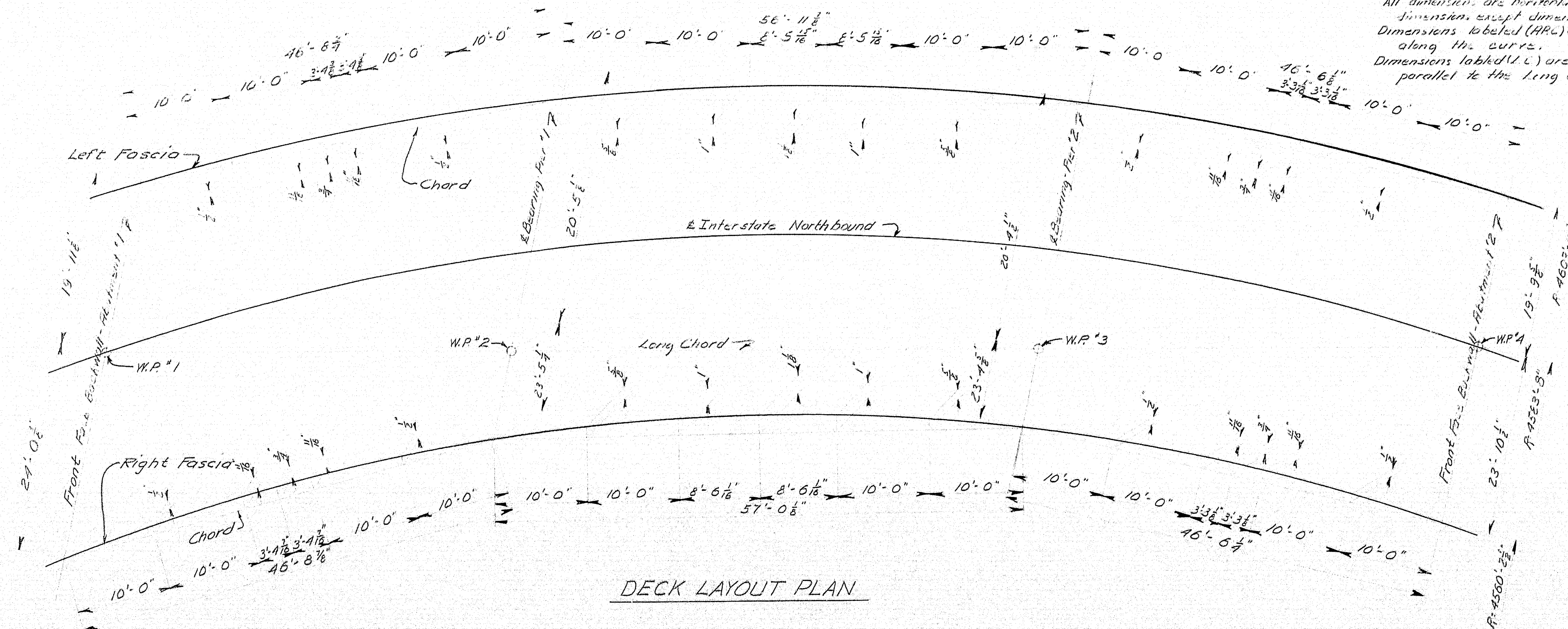


PLAN

NOTE:

All dimensions are horizontal and are based on dimensions except dimensions labeled (H.C.) are measured along the chord. Dimensions labeled (L.C.) are measured parallel to the long chord.

Family marked (C) are to be cast first, no panels marked (C) shall be cast until panels (C) are completed. C.T. - Construction Joint.

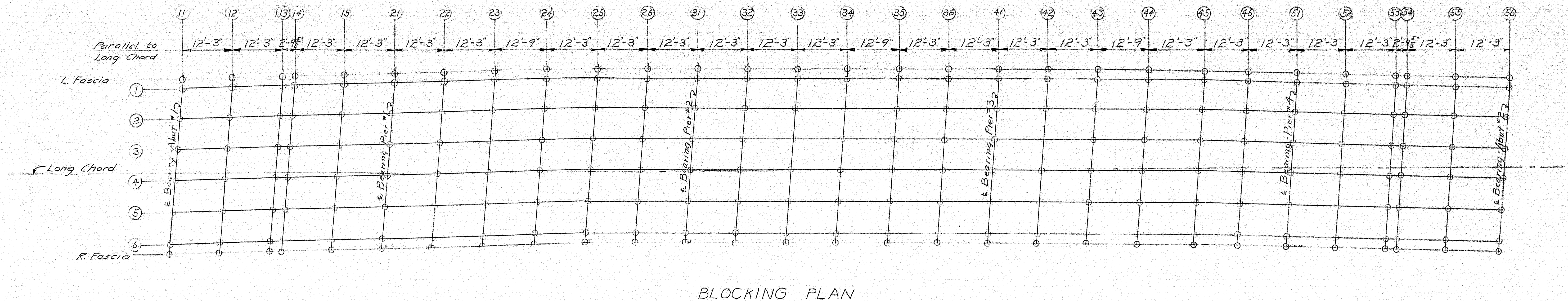


DECK LAYOUT PLAN

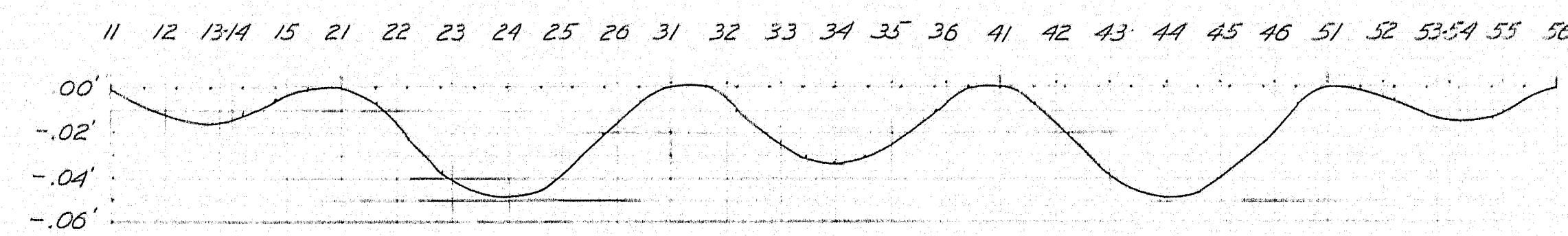
NOTES:

1. For General Notes see Sheet 33.
2. For Details and Sections see Sheet 35.

DESIGN - P.W. TRACE - J.H. CHECK - C.H.	BRIDGE NO. SURVEY PLOT
STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER MAINE CENTRAL RAILROAD & ROUTE 152 IN THE TOWN OF PITTSFIELD SOMERSET COUNTY SUPERSTRUCTURE - ROUTE 152 - NORTHBOUND SHEET 34 OF 41 AUGUSTA, MAINE JAN. 63	

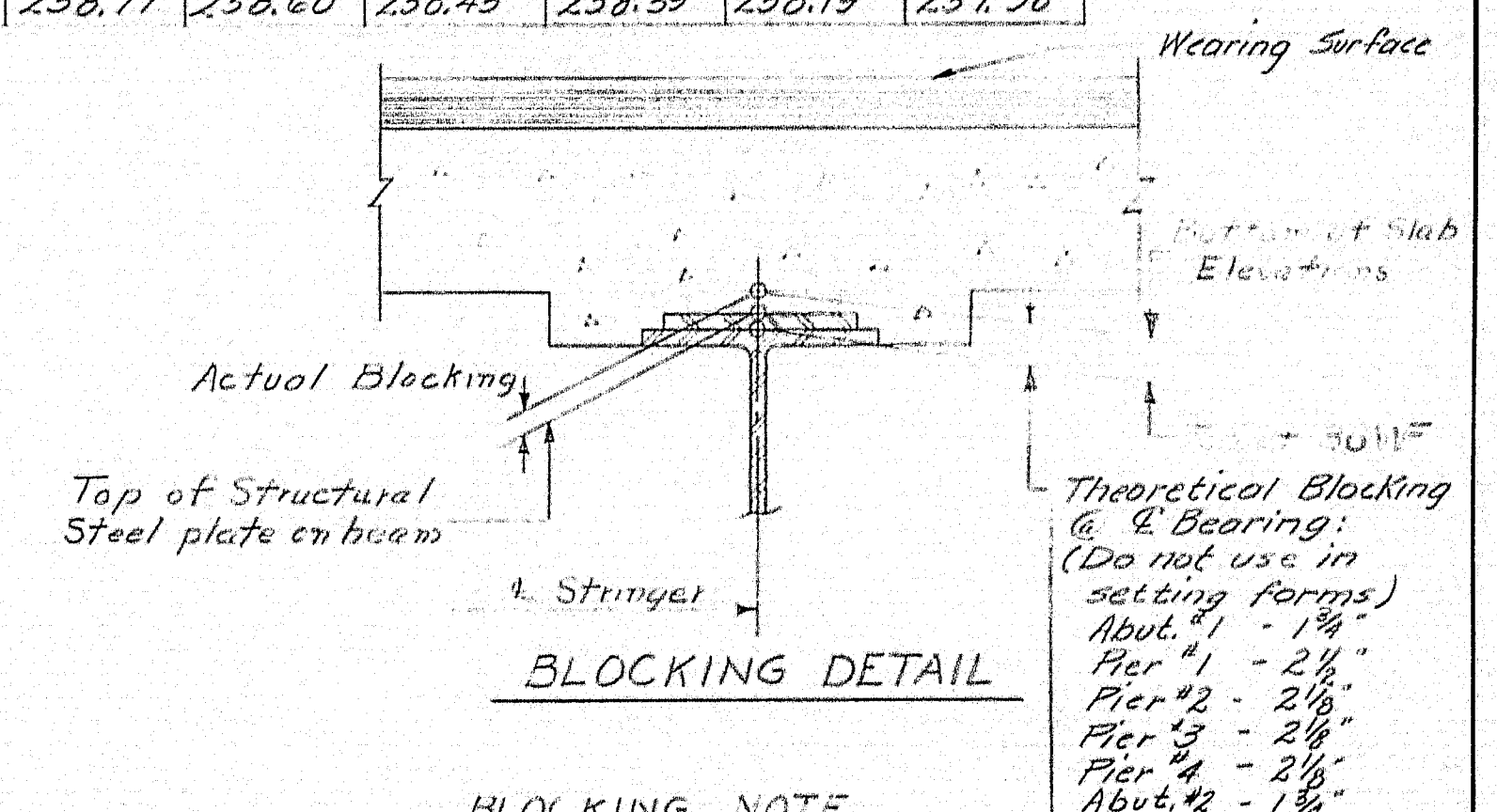


BOTTOM OF SLAB ELEVATIONS																													
POINT	11	12	13	14	15	21	22	23	24	25	26	31	32	33	34	35	36	41	42	43	44	45	46	51	52	53	54	55	56
L. Fascia	261.51	261.54	261.55	261.55	261.54	261.53	261.52	261.52	261.49	261.44	261.36	261.28	261.22	261.16	261.08	260.97	260.84	260.72	260.61	260.51	260.37	260.21	260.04	259.95	259.68	259.51	259.46	259.27	259.05
1	261.52	261.54	261.56	261.56	261.54	261.53	261.53	261.53	261.50	261.44	261.37	261.28	261.22	261.15	261.07	260.96	260.84	260.72	260.62	260.51	260.38	260.23	260.05	259.86	259.69	259.52	259.48	259.28	259.07
2	261.30	261.33	261.34	261.34	261.33	261.31	261.31	261.31	261.29	261.24	261.15	261.07	261.01	260.94	260.86	260.75	260.63	260.51	260.41	260.30	260.17	260.02	259.84	259.65	259.48	259.31	259.27	259.07	258.86
3	261.08	261.11	261.12	261.13	261.11	261.10	261.10	261.10	261.08	261.02	260.94	260.86	260.79	260.73	260.65	260.55	260.42	260.30	260.20	260.09	259.96	259.81	259.63	259.44	259.27	259.10	259.06	258.86	258.65
4	260.87	260.90	260.91	260.91	260.90	260.89	260.89	260.89	260.86	260.81	260.73	260.65	260.58	260.52	260.44	260.34	260.21	260.09	259.99	259.89	259.75	259.60	259.42	259.24	259.06	258.89	258.85	258.65	258.44
5	260.65	260.68	260.68	260.70	260.69	260.67	260.68	260.68	260.65	260.60	260.52	260.44	260.37	260.31	260.23	260.13	260.00	259.88	259.78	259.68	259.55	259.39	259.21	259.03	258.85	258.68	258.64	258.45	258.23
6	260.44	260.47	260.48	260.48	260.47	260.46	260.46	260.47	260.44	260.39	260.31	260.23	260.16	260.10	260.02	259.92	259.79	259.68	259.57	259.47	259.34	259.19	259.00	258.82	258.65	258.47	258.43	258.24	258.02
R. Fascia	260.40	260.42	260.44	260.44	260.43	260.42	260.42	260.42	260.39	260.34	260.27	260.19	260.13	260.07	259.99	259.88	259.76	259.64	259.53	259.43	259.29	259.14	258.96	258.77	258.60	258.43	258.39	258.19	257.98



DEFLECTION CURVE

Deflection for Dead Load minus weight of Stringer



BLOCKING NOTE

In order that the Wearing Surface may conform to the profile and cross-sections shown on these plans, the accompanying tables of Bottom of Slab Elevations are given. These elevations, which are computed to compensate for dead load deflections must be set before slab forms are to be started.

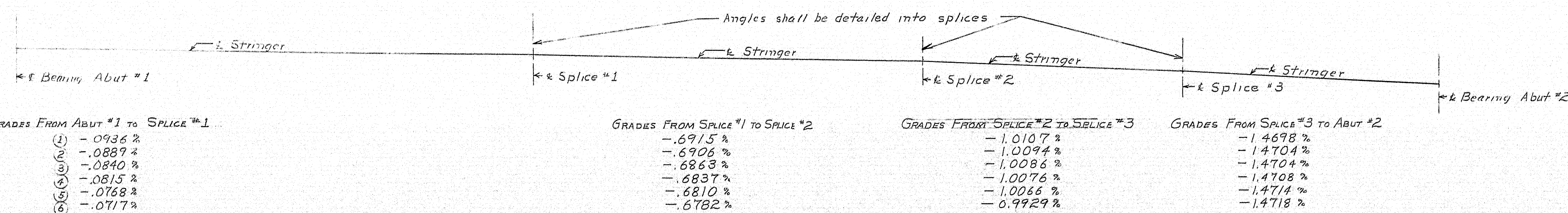


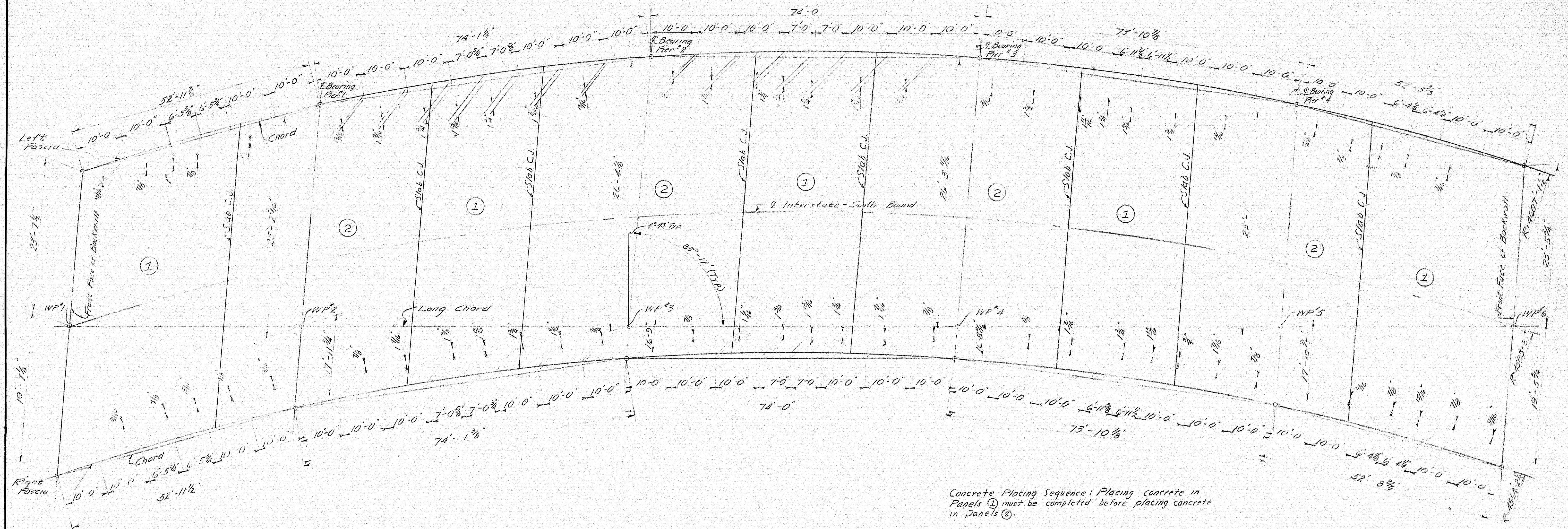
DIAGRAM OF GRADES AT & OF STRINGERS

DESIGN-C.D.H.
SURVEY-L.E.C.
CHECK-KAB

BRIDGE NO.
SURVEY-
PLOT-

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95
OVER
MAINE CENTRAL RAILROAD
& **ROUTE 152**
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY

BLOCKING DETAILS-SOUTHBOUND
SHEET 35 OF 41 AUGUSTA, MAINE JAN 63

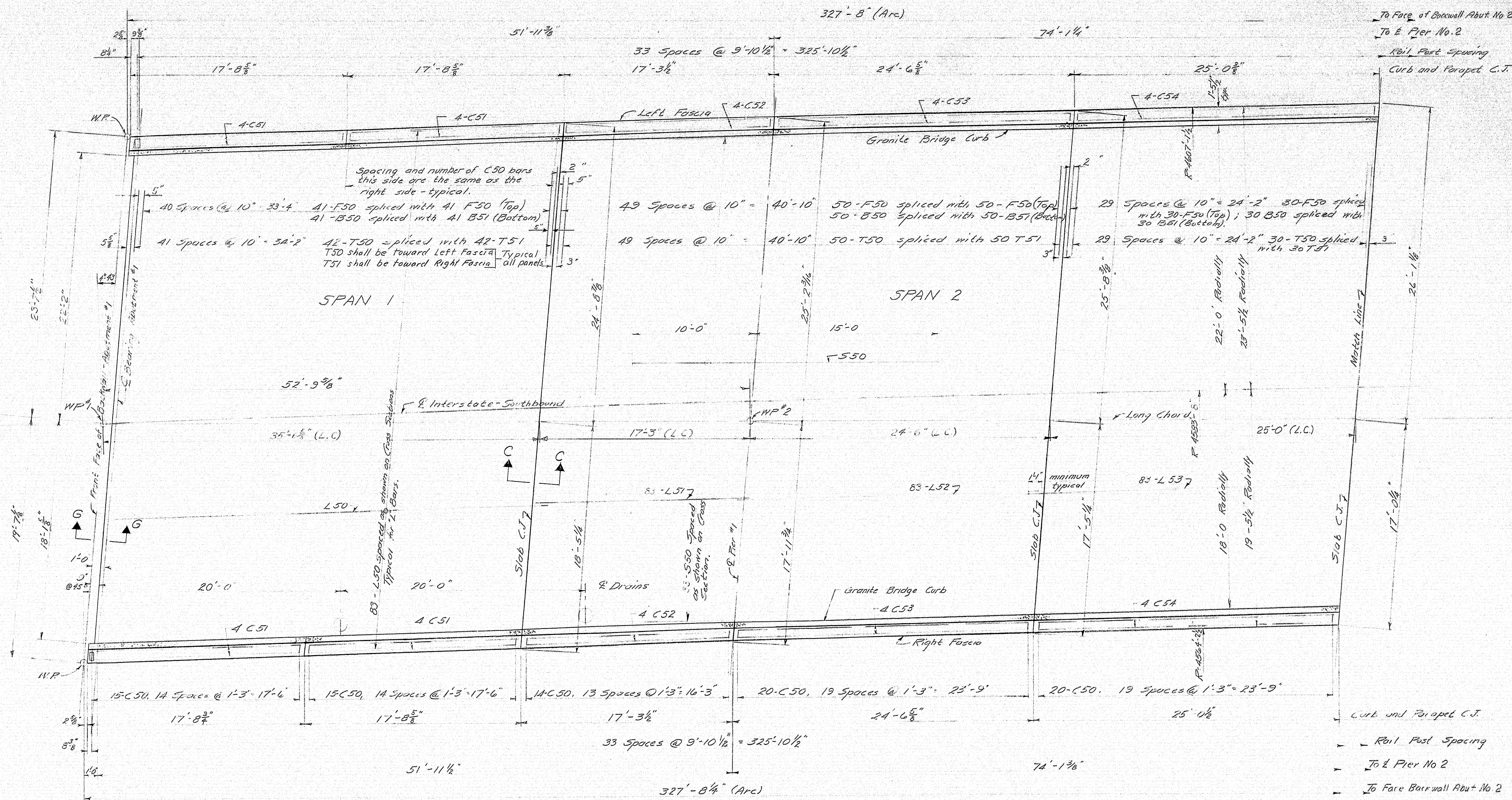


DECK LAYOUT PLAN

DESIGN - C. O. H. TRACE - P. A. H. CHECK - 1/1/12	BRIDGE NO. PLOT - 1/1/12
STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER MAINE CENTRAL RAILROAD & ROUTE 152 IN THE TOWN OF PITTSFIELD SOMERSET COUNTY DECK LAYOUT - SOUTHBOUND SHEET 36 OF 41 AUGUSTA, MAINE JAN. 63	



SHEET NO.	STATE	PROJECT NUMBER	SHEET TOTAL
37	MAINE	2-95-7146	41



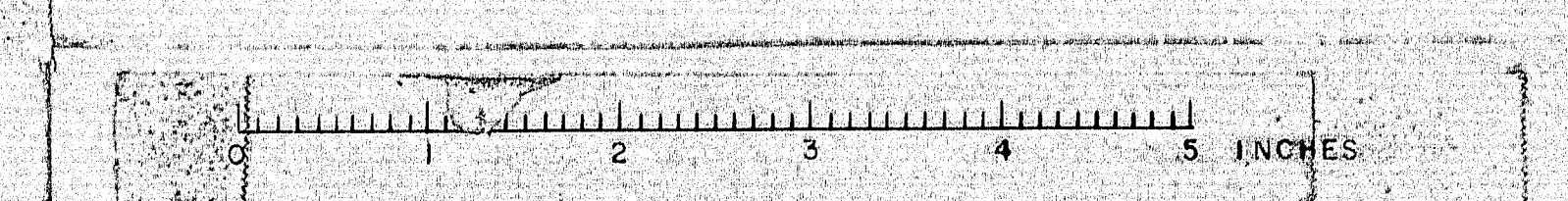
PLAN

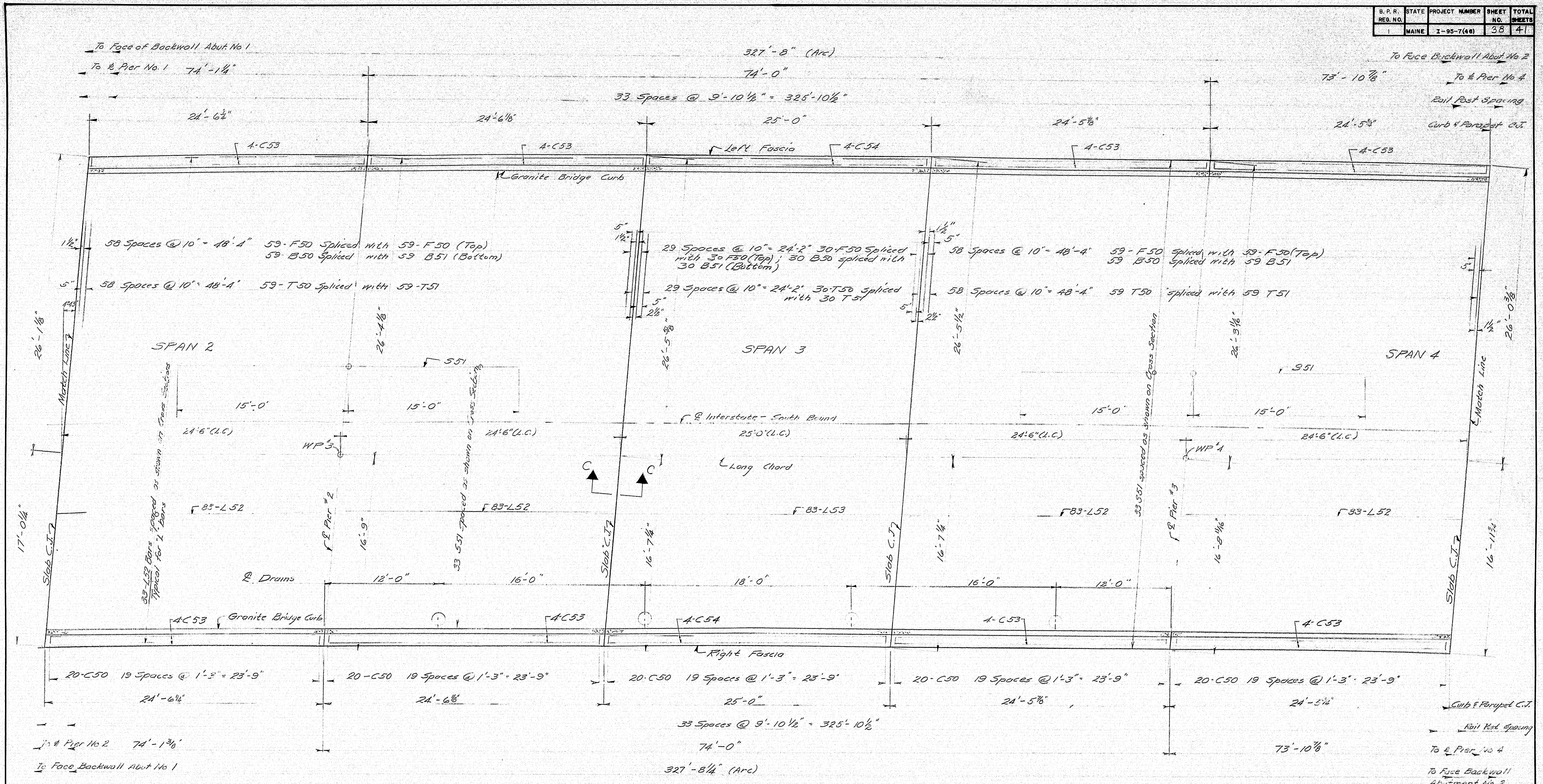
NOTES

All dimensions are horizontal and are chord dimensions except those marked (L.C.) or (Arc). Dimensions labeled (Arc) are measured along the appropriate curve. Dimensions labeled (L.C.) are measured parallel to the Long Chord.

For General Notes see Sh. 29
For Details and Sections see Sh. 30 & 29.
For Deck Layout Plan and Concrete Placement Sequence, see Sh. 36
For Expansion Dam Details see Sh. 29

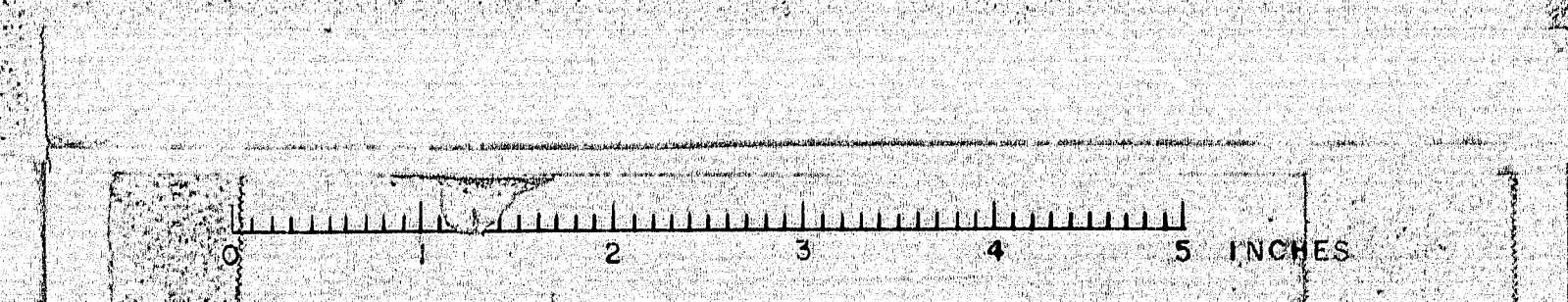
DESIGN - C. D. H. DETAIL - P. L. A. CHECK - P. L. A.	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER MAINE CENTRAL RAILROAD & ROUTE 152 IN THE TOWN OF PITTSFIELD SOMERSET COUNTY SUPERSTRUCTURE - SPANS 1, 2 - SOUTHBOUND SHEET 37 OF 41 AUGUSTA, MAINE JAN. 63	

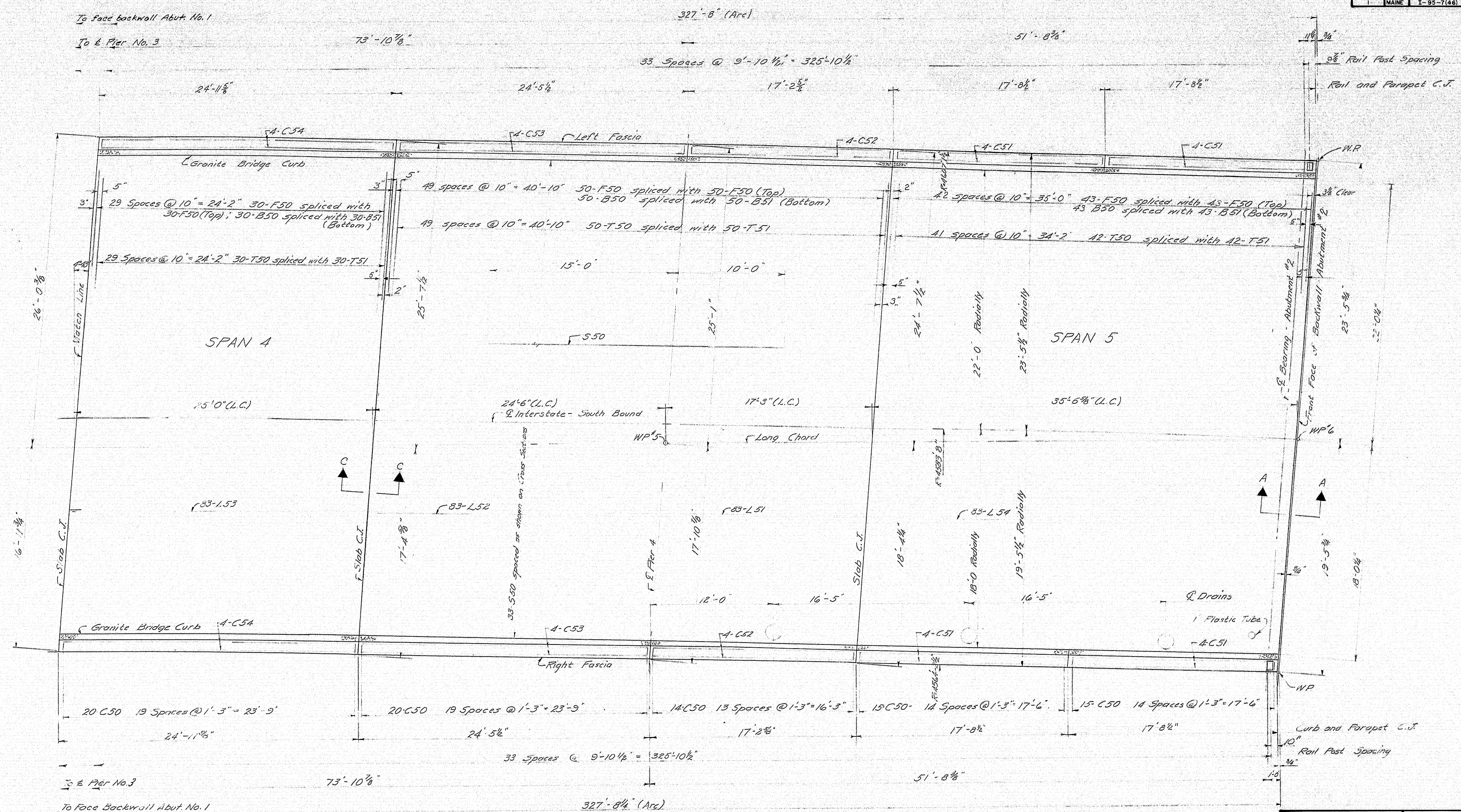




NOTE:
 All dimensions are horizontal and are chord dimensions except those labeled (Arc) or (L.C.).
 Dimensions labeled (Arc) are measured along the appropriate curve.
 Dimensions labeled (L.C.) are measured parallel to the long chord.
 For General Notes see Sh. 29.
 For Details and Sections see Sh. 30 & 29.
 For Deck Layout Plan and concrete placement sequence see Sheet 36.

DESIGN - C.D.H. DETAIL - P.L.A. CHECK - <i>Poten</i>	BRIDGE NO. PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER	
MAINE CENTRAL RAILROAD & ROUTE 152	
IN THE TOWN OF PITTSFIELD SOMERSET COUNTY	
SUPERSTRUCTURE - SPANS 2, 3 & 4 - SOUTHBOUND	
SHEET 38 OF 41	AUGUSTA, MAINE JAN. 63





PLAN

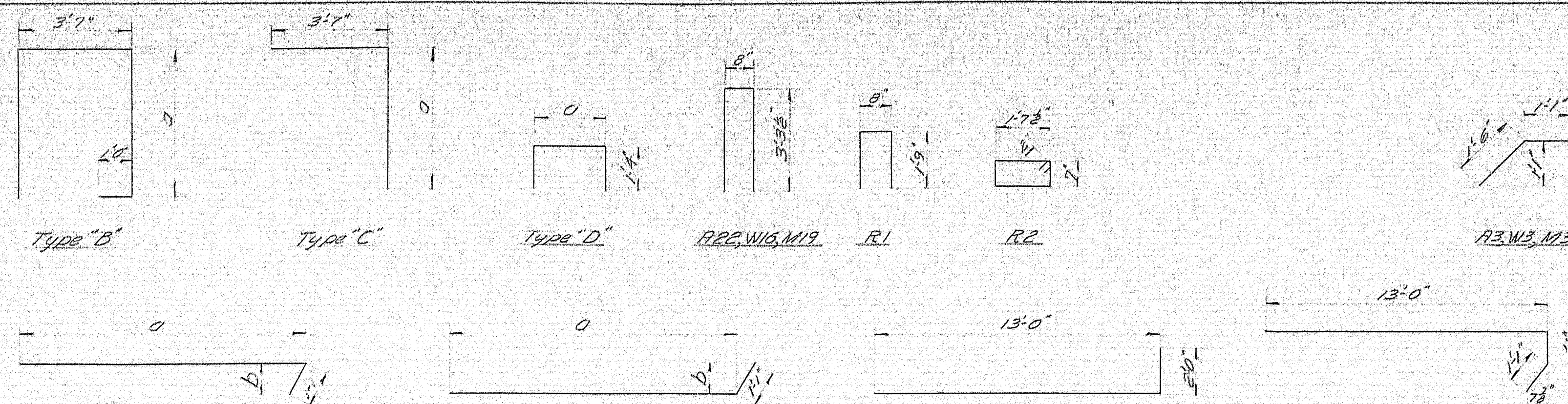
NOTE
 All dimensions are horizontal and are chord dimensions except those labeled (Arc) or (L.C.).
 Dimensions labeled (Arc) are measured along the appropriate curve.
 Dimensions labeled (L.C.) are measured parallel to the Long Chord.
 For General Notes see Sheet 29.
 For Details and Sections see Sheet 30 & 29.
 For Deck Layout Plan and concrete placement sequence see Sheet 36.

DESIGN - C. D. H.	BRIDGE NO.
DETAIL - P. L. A.	SURVEY -
CHECK - <i>Peter</i>	PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER	
MAINE CENTRAL RAILROAD & ROUTE 152	
IN THE TOWN OF PITTSFIELD	
SOMERSET COUNTY	
SUPERSTRUCTURE - SPANS 4 & 5 - SOUTHBOUND	
SHEET 39 OF 41	AUGUSTA, MAINE JAN 63

REINFORCING STEEL SCHEDULE - ABUTMENTS

B. P. R. REG. NO. 1 STATE MAINE PROJECT NUMBER J-25-7(46) SHEET NO. 40 TOTAL SHEETS 41

BENT BARS



Mark	Size	No.	Length	Type	Dimensions	Location	Mark	Size	No.	Length	Type	Dimensions	Location
A3	#4	65	2'7"			Approach Slab	W29	#5	18	4'2"	"D"	1'5 3/8"	Bearing Pad
A15	#5	29	16'9"	"B"	4'1"	Bridge Seat	W34	#4	24	15'7"	"B"	5'6"	Bridge Seat
A18	#5	28	14'1"	"B"	4'1"	do	W36	#4	18	14'7"	"B"	5'0"	do
A19	#5	3	8'2"	"C"	4'7"	do	W37	#5	2	7'10"	"C"	4'3"	do
A22	#4	52	7'3"			Wing Wall	W3	#6	52	2'7"			Approach Slab
A28	#5	6	15'7"	"F"	14'6" 13 3/4"	do	M11	#5	24	13'3"	"B"	4'4"	Bridge Seat
A30	#4	8	10'6"	"E"	16'5" 10 3/8"	do	M13	#5	18	14'7"	"B"	5'0"	do
A31	#4	8	15'2"			do	M14	#5	2	8'7"	"C"	5'0"	do
A32	#4	8	15'0"			do	M19	#4	56	7'3"			Wing Wall
A33	#2	5'4"	"D"	1'8"		Bearing Pad	M20	#5	6	16'11"	"F"	15'10" 10 3/8"	do
A34	#4	18	4'2"	"D"	1'5 3/8"	do	M22	#4	8	14'11"	"F"	13'10" 10 3/8"	do
A44	#28	1'3"	"E"	5'7"		Bridge Seat	M26	#4	6	16'11"	"E"	15'10" 10 3/8"	do
A45	#28	4'3"	"B"	5'1"		do	M28	#4	8	14'11"	"E"	13'10" 1'0 3/8"	do
A49	#5	3	8'4"	"C"	4'3"	do	M29	#4	12	5'4"	"D"	2'7 3/8"	Bearing Pad
W3	#6	54	2'7"			Approach Slab	M30	#4	18	4'6"	"D"	1'5 3/8"	do
W11	#5	24	13'3"	"B"	4'4"	Bridge Seat	M42	#4	24	15'9"	"B"	5'6"	Bridge Seat
W13	#5	18	14'5"	"B"	4'11"	do	M44	#5	20	14'4"	"B"	4'10"	do
W14	#5	2	8'5"	"C"	4'10"	do							
W16	#4	52	7'3"			Wing Wall	R1	#4	24	4'2"			End Post
W22	#5	6	16'8"	"E"	15'7" 10 3/8"	do	R2	#4	12	4'9"			do
W24	#4	8	14'8"	"E"	13'7" 10 3/8"	do							
W25	#4	6	16'6"	"F"	15'5" 10 3/8"	do							
W27	#4	8	4'5"	"F"	13'4" 10 3/8"	do							
W28	#5	12	5'4"	"D"	2'7 3/8"	Bearing Pad							

STRAIGHT BARS

Mark	Size	No.	Length	Location	Mark	Size	No.	Length	Location
A1	#4	40	24'9"	Approach Slab	W33	#6	39	6'0"	Footings Abut. 2
A2	#6	194	18'6"	do	W35	#5	84	4'8"	Back Wall
A4	#4	47	5'0"	Footings-Abut. 1	W38	#4	28	6'5"	Wing Wall
A5	#4	12	28'5"	Footings-Abut. 1	W39	#4	2	4'10"	do
A7	#4	1	15'8"	Footings-Abut. 1	W40	#4	28	7'4"	do
A8	#4	1	16'4"	do	W41	#4	2	5'9"	do
A9	#4	1	17'1"	do	W42	#5	24	22'2"	Breast Wall & Back Wall
A10	#4	1	17'10"	do	W43	#6	10	22'9"	Bridge Seat
A11	#4	1	16'6"	do	W44	#5	4	12'8"	Wing Wall
A12	#4	1	17'3"	do	W45	#4	4	13'2"	do
A13	#4	1	18'0"	do	W46	#4	4	12'4"	do
A14	#6	1	18'9"	do	W47	#5	4	13'0"	do
A16	#5	58	5'7"	Back Wall	M1	#4	80	19'9"	Approach Slab
A17	#4	44	5'6"	do	M2	#6	310	14'8"	do
A20	#4	28	7'4"	Wing Wall	M4	#4	38	5'0"	Footings
A21	#4	28	6'6"	do	M5	#5	12	23'9"	do
A23	#4	1	4'2"	do	M7	#4	2	17'1"	do
A24	#4	2	5'0"	do	M8	#4	2	17'4"	do
A25	#5	24	27'5"	Breast Wall & Back Wall	M9	#4	2	17'2"	do
A26	#6	10	28'0"	Bridge Seat	M10	#6	2	16'11"	do
A29	#7	28	13'1"	Wing Wall	M12	#5	34	6'8"	Back Wall
A35	#5	8	12'8"	do	M15	#5	24	22'0"	Breast Wall & Back Wall
A36	#5	8	12'1"	do	M16	#6	10	22'6"	Bridge Seat
A41	#4	40	24'0"	Approach Slab	M17	#5	28	7'10"	Wing Wall
A42	#6	190	18'0"	do	M18	#5	2	4'9"	do
A43	#6	14	29'2"	Footings	M21	#7	14	15'10"	do
A46	#5	100	5'3"	Back Wall	M23	#5	8	13'5"	do
A47	#6	10	27'3"	Bridge Seat	M24	#5	28	8'11"	do
A48	#5	24	26'9"	Breast Wall & Back Wall	M25	#5	2	5'10"	do
A50	#4	2	4'3"	Wing Wall	M27	#7	14	13'9"	do
A51	#4	28	6'2"	do	M31	#5	4	13'0"	do
A52	#4	28	7'1"	do	M33	#5	4	12'8"	do
A53	#5	1	5'1"	do	M41	#6	14	23'7"	Footings
W1	#4	40	20'1"	Approach Slab	M43	#5	88	5'5"	Back Wall
W2	#6	150	15'1"	do	M45	#5	24	21'10"	Breast Wall & Back Wall
W4	#4	39	5'2"	Footings-Abut. 1	M46	#6	34	5'6"	Footings
W5	#4	26	24'1"	Footings-Abut. 1 & 2	M47	#6	4	17'1"	do
W7	#4	4	16'9"	Footings	M48	#6	4	15'8"	do
W8	#4	4	16'11"	do	M47	#5	28	8'3"	Wing Wall
W9	#4	4	17'2"	do	M50	#5	4	5'6"	do
W10	#6	4	17'4"	do	M51	#5	28	7'2"	do
W12	#5	84	5'6"	Back Wall	M52	#6	18	22'4"	Bridge Seat
W15	#4	28	7'8"	Wing Wall	M53	#5	112	3'0"	Footings dowels
W17	#4	2	5'5"	do	A63	#5	112	3'0"	Footings dowels
W18	#4	28	6'7"	do	A6	#6	56	3'6"	Footings
W19	#4	2	4'6"	do	W6	#6	56	3'6"	Footings
W20	#5	24	22'4"	Breast Wall & Back Wall	M6	#6	56	3'6"	Footings
W21	#6	10	22'11"	Bridge Seat					
W23	#7	14	13'2"	Wing Wall					
W26	#7	14	13'5"	do					
W31	#4	40	20'0"	Approach Slab					
W32	#6	158	14'11"	do					

STRAIGHT BARS

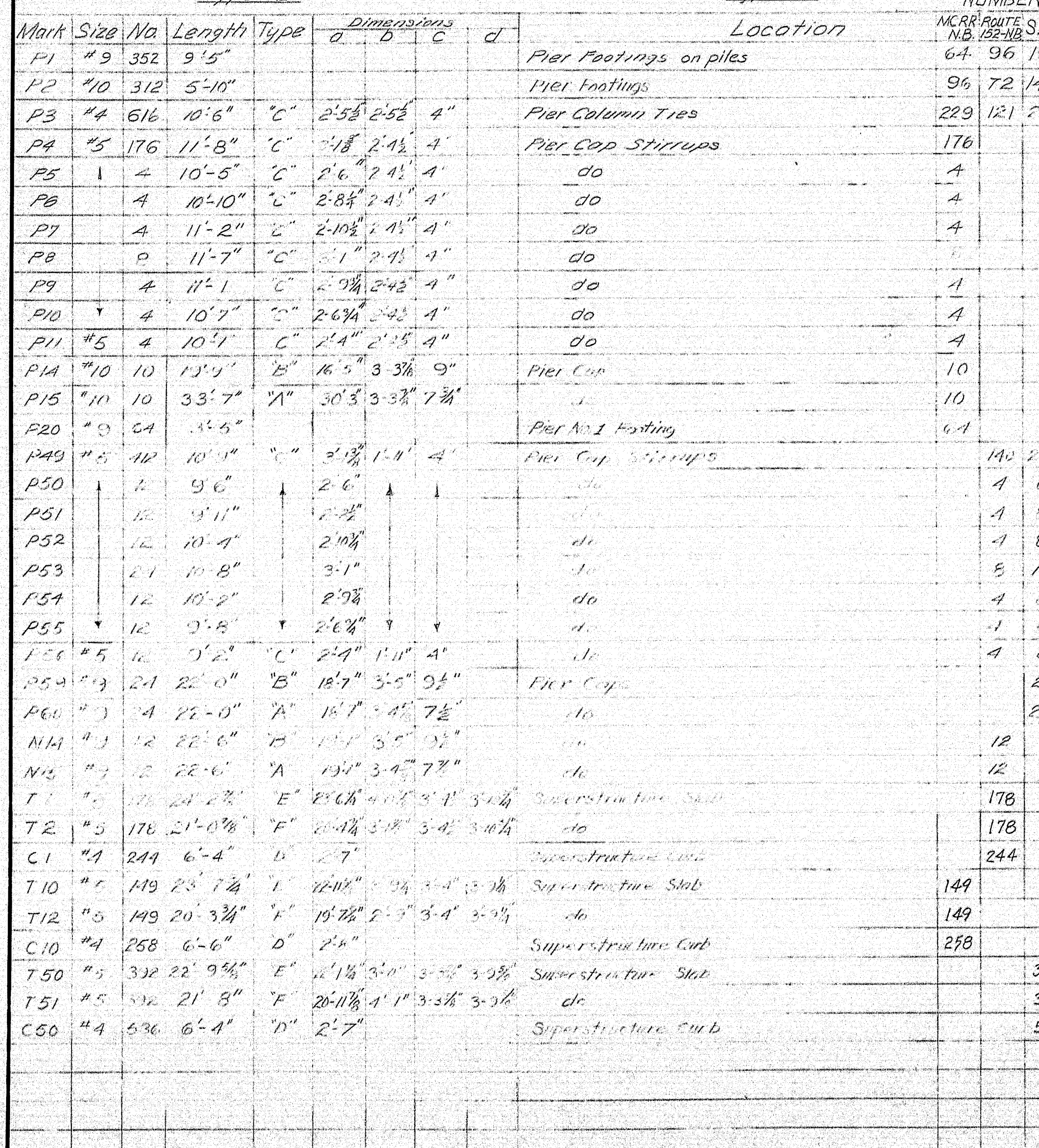
Mark	Size	No.	Length	Location
A54	#6	47	6'0"	Footings-Abut. 2
A55		1	16'10"	do
A56		1	17'6"	do
A57		1	18'3"	do
A58		1	19'0"	do
A59		1	17'9"	do
A60		1	18'6"	do
A61		1	19'2"	do
A62		1	19'11"	do
W48		37	6'2"	Footings-Abut. 2
W49		2	17'9"	do
W50		2	17'11"	do
W51		2	18'2"	do
W52	↓	2	18'4"	do
W53	#5	112	3'0"	Footings dowels

NOTES

1. All bars shall be Intermediate Grade Steel.
2. All dimensions are to the centerline of the bars.
3. A-bars are for the M.C.R.R. bridge, Northbound.
4. W-bars are for the Route 152 bridge, Northbound.
5. M-bars are for the Southbound bridge.
6. E1 and E2 bars are for all abutments.

DESIGN-C.D.H. DETAIL-L.L.R. BRIDGE NO. SURVEY-PLOT-
 CHECK-MAR. Dressell
 STATE HIGHWAY COMMISSION
 BRIDGE DIVISION
INTERSTATE 95
 OVER
MAINE CENTRAL RAILROAD & ROUTE 152
 IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY
 REINFORCING STEEL SCHEDULE - ABUTMENTS
 SHEET 40 OF 41 AUGUSTA, MAINE JAN. 1963

B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-7(96)	4	4



NUMBER						STRAIGHT BARS						NUMBER					
Mark	Size	No.	Length	Location	M.C.R. Route N.B. 152-118 S.B.	Mark	Size	No.	Length	Location	M.C.R. Route N.B. 152-118 S.B.	Mark	Size	No.	Length	Location	M.C.R. Route N.B. 152-118 S.B.
P13	#6	16	26'-5"	Pier Caps	16	F1	#5	358	22'-4"	Slab	358	F41	#5	8	35'-8"	Slab	8
P16	#8	20	27'-5"	do	20	B1	1	179	26'-2"	do	179	F42	1	9	36'-10"	do	9
P17	#8	16	9'-6"	do	16	B2		179	18'-7"	do	179	F43		8	37'-11"	do	8
P18	#10	24	34'-5"	Pier Columns	24	L1		166	27'-2"	do	166	F44		8	39'-0"	do	8
P19	#10	48	26'-9"	do	48	L2		83	26'-6"	do	83	F45		8	40'-0"	do	8
P21	#10	12	32'-5"	do	12	L3	1	166	36'-6"	do	166	F46	308		21'-10"	do	308
P22	#10	12	30'-5"	do	12	S1	#5	66	16'-0"	do	66	B10		150	17'-6"	do	150
						C2	#4	16	25'-6"	Curb	16	B12		150	26'-2"	do	150
						C3	1	16	20'-6"	do	16	L10	1	332	39'-9"	do	332
						C4	1	16	15'-6"	do	16	S10	#5	66	16'-0"	do	66
						C5	#4	8	24'-3"	do	8	C11	#4	8	16'-8"	Curb	8
P58	#6	32	21'-5"	Pier Caps	32	F11	#5	8	2'-4"	Slab	8	C12	1	4	20'-8"	do	4
P61	#9	24	14'-8"	do	24	F12	1	5	3'-5"	do	5	C13		4	8'-8"	do	4
P62	#7	24	30'-6"	do	24	F13		8	4'-6"	do	8	C14		4	12'-8"	do	4
P63	#10	32	9'-10"	do	32	F14		8	5'-8"	do	8	C15		8	21'-7"	do	8
P64	#9	36	25'-3"	Pier Columns	36	F15		8	6'-5"	do	8	C16		4	10'-0"	do	4
P65	1	36	25'-6"	do	36	F16		8	7'-10"	do	8	C17		4	21'-4"	do	4
P66	1	36	24'-6"	do	36	F17		8	9'-0"	do	8	C18		4	17'-5"	do	4
P67	#9	36	22'-5"	do	36	F18		8	10'-1"	do	8	C19		4	25'-3"	do	4
P68	#8	16	8'-5"	Pier Caps	16	F19		16	11'-2"	do	16	C20		4	12'-1"	do	4
						F20		8	12'-4"	do	8	C21		8	20'-0"	do	8
						F21		8	13'-5"	do	8	C22		4	7'-10"	do	4
						F22		8	14'-6"	do	8	C23	1	4	22'-6"	do	4
						F23		8	15'-8"	do	8	C24	#4	8	15'-6"	do	8
						F24		8	16'-0"	do	8	F50	#5	764	22'-0"	Slab	764
						F25		8	17'-11"	do	8	B50	1	392	25'-9"	do	392
						F26		8	19'-0"	do	8	B51	392		18'-3"	do	392
N13	#6	16	21'-11"	Pier Caps	16	F27		8	20'-11"	do	8	L50	50		35'-11"	do	83
N16	#8	12	15'-0"	do	12	F28		8	21'-3"	do	8	L51		8	17'-9"	do	166
N17	#8	12	30'-8"	do	12	F29		8	22'-4"	do	8	L52		4	11'-0"	do	498
N18	#8	16	8'-6"	do	16	F30		8	23'-5"	do	8	L53		4	27'-5"	do	249
N19	#10	36	23'-2"	Pier Columns	36	F31		8	24'-7"	do	8	L54		8	36'-8"	do	83
N20	#10	36	20'-7"	do	36	F32		8	25'-6"	do	8	S50	1	66	25'-0"	do	66
N21	#8	8	5'-0"	Pier Cap	8	F33		8	26'-10"	do	8	S51	#5	66	30'-0"	do	66
						F34		8	27'-11"	do	8	C51	#4	32	17'-4"	Curb	32
						F35		8	29'-0"	do	8	C52	1	16	16'-4"	do	16
						F36		8	30'-11"	do	8	C53	1	48	24'-1"	do	48
						F37		8	31'-3"	do	8	C54	#4	24	24'-7"	do	24
						F38		8	32'-4"	do	8						
						F39	1	8	33'-6"	do	8						
						F40	#5	8	34'-7"	do	8						

NOTES

1. All bars shall be Intermediate Grade Steel.
2. All dimensions are to the centerline of the bars.

Piers & Superstr. NB	✓ Coffin
Piers N.B.	✓ Dreselly
Superstr. S.B.	✓ Daten

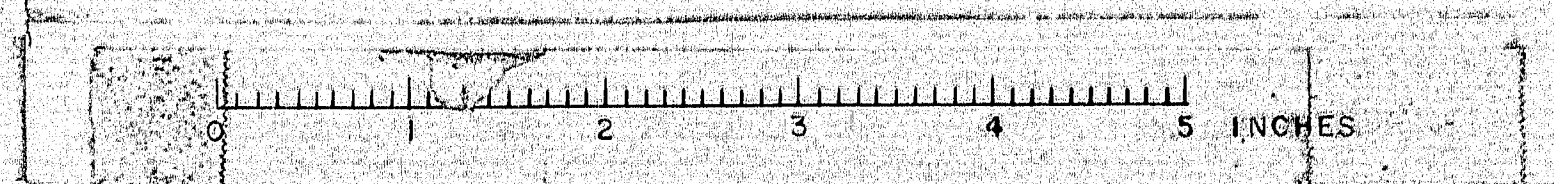
DESIGN—	C.D.H.	DETAIL—P.L.A.	BRIDGE NO.
TRACE—			SURVEY—
CHECK—	<i>as noted</i>		PLOT—

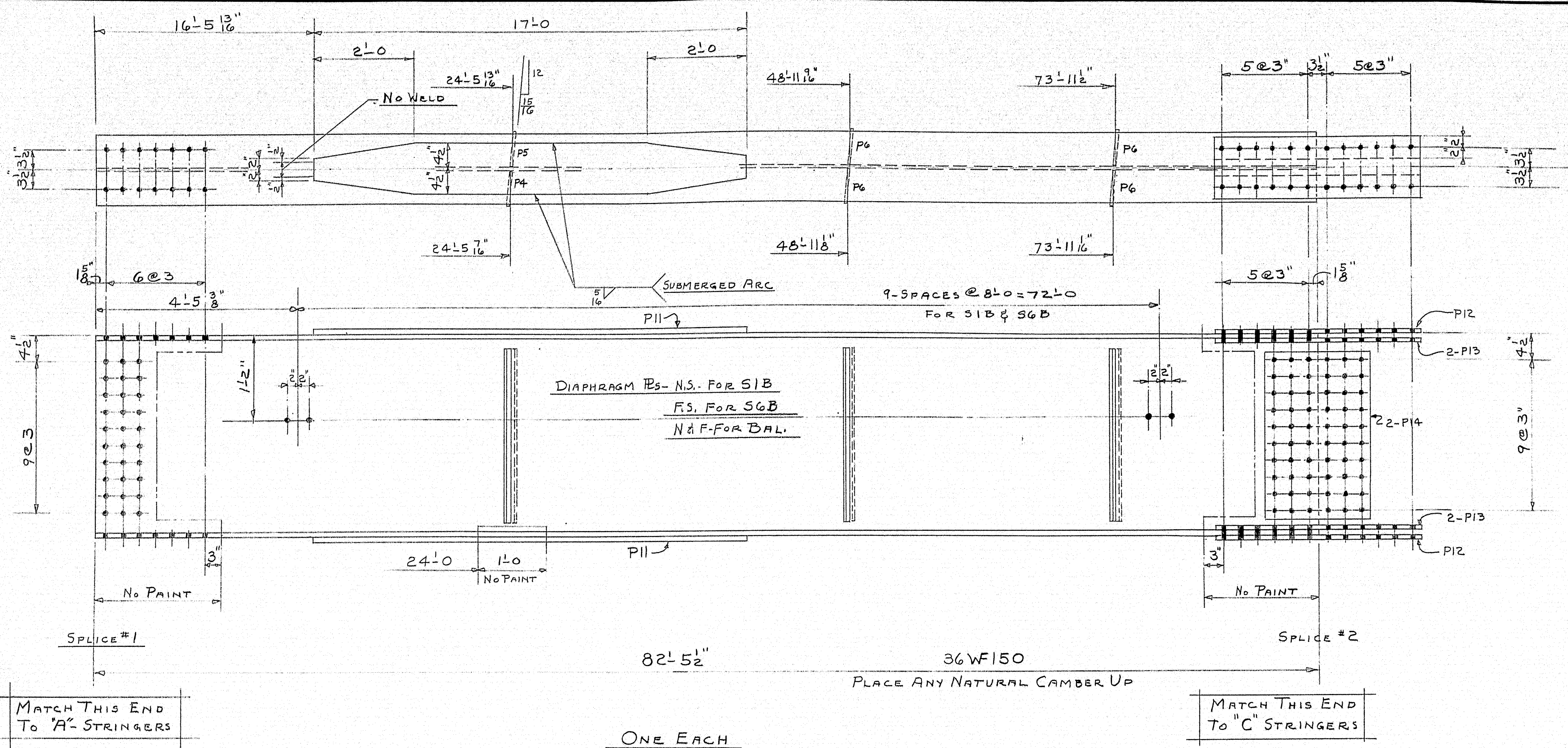
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95
OVER
**MAINE CENTRAL RAILROAD
& ROUTE 152**

IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY

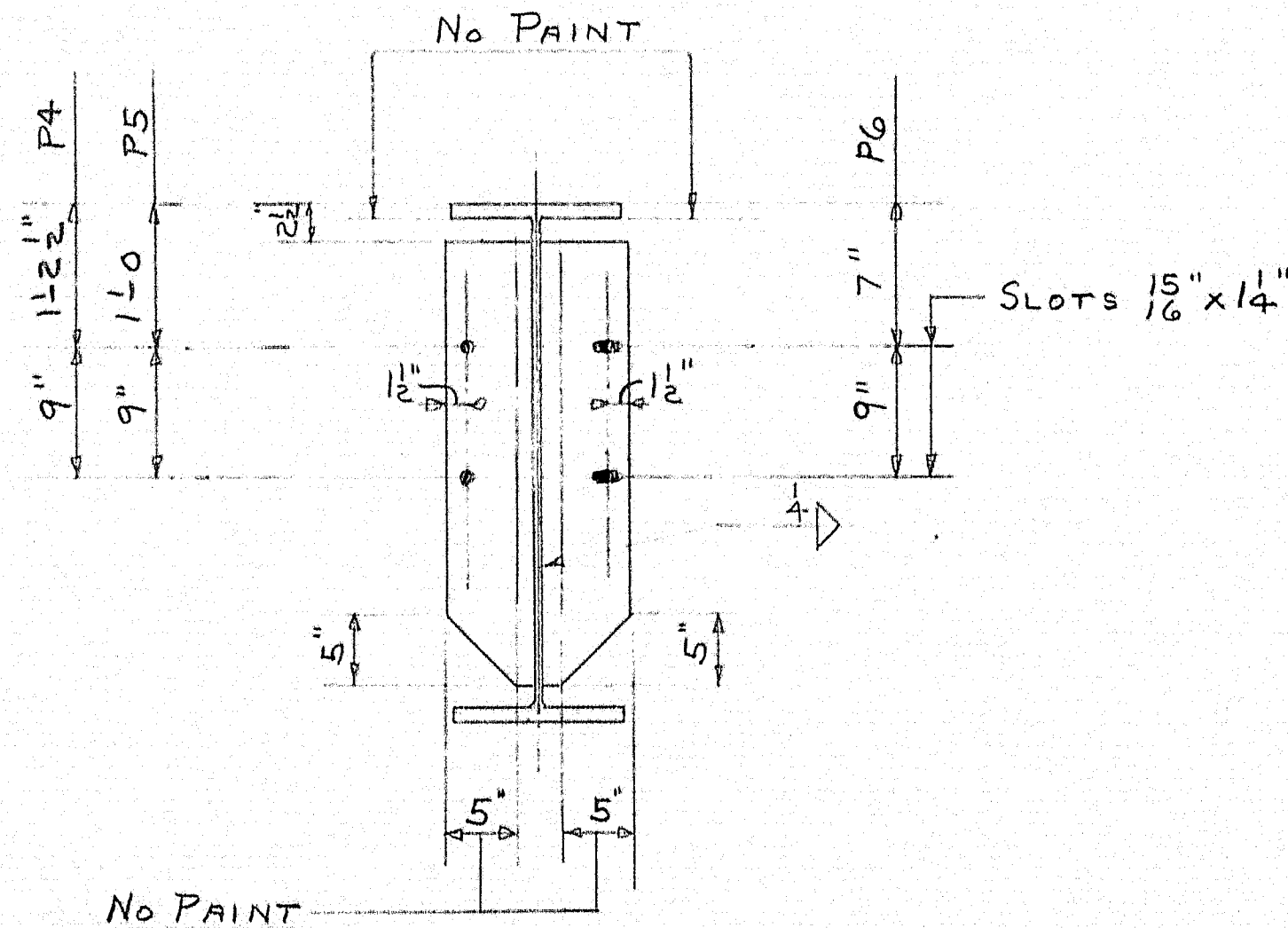
REINFORCING STEEL SCHEDULE—PIERS & SUPERSTRUC.
SHEET #1 OF 41 AUGUSTA, MAINE JAN. 1963





ONE EACH
S1B-S2B-S3B-S4B-S5B-S6B

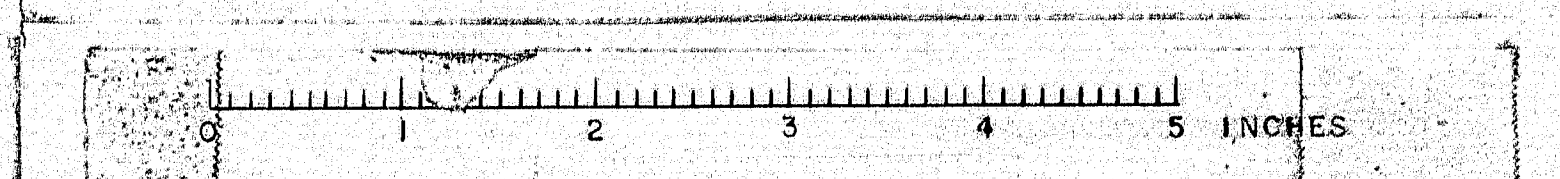
NOTE:
SET STRINGERS IN ACCORDANCE WITH THE DIAGRAMS
SHOWN ON DWG. B63-188-E2 FOR MATCHING.

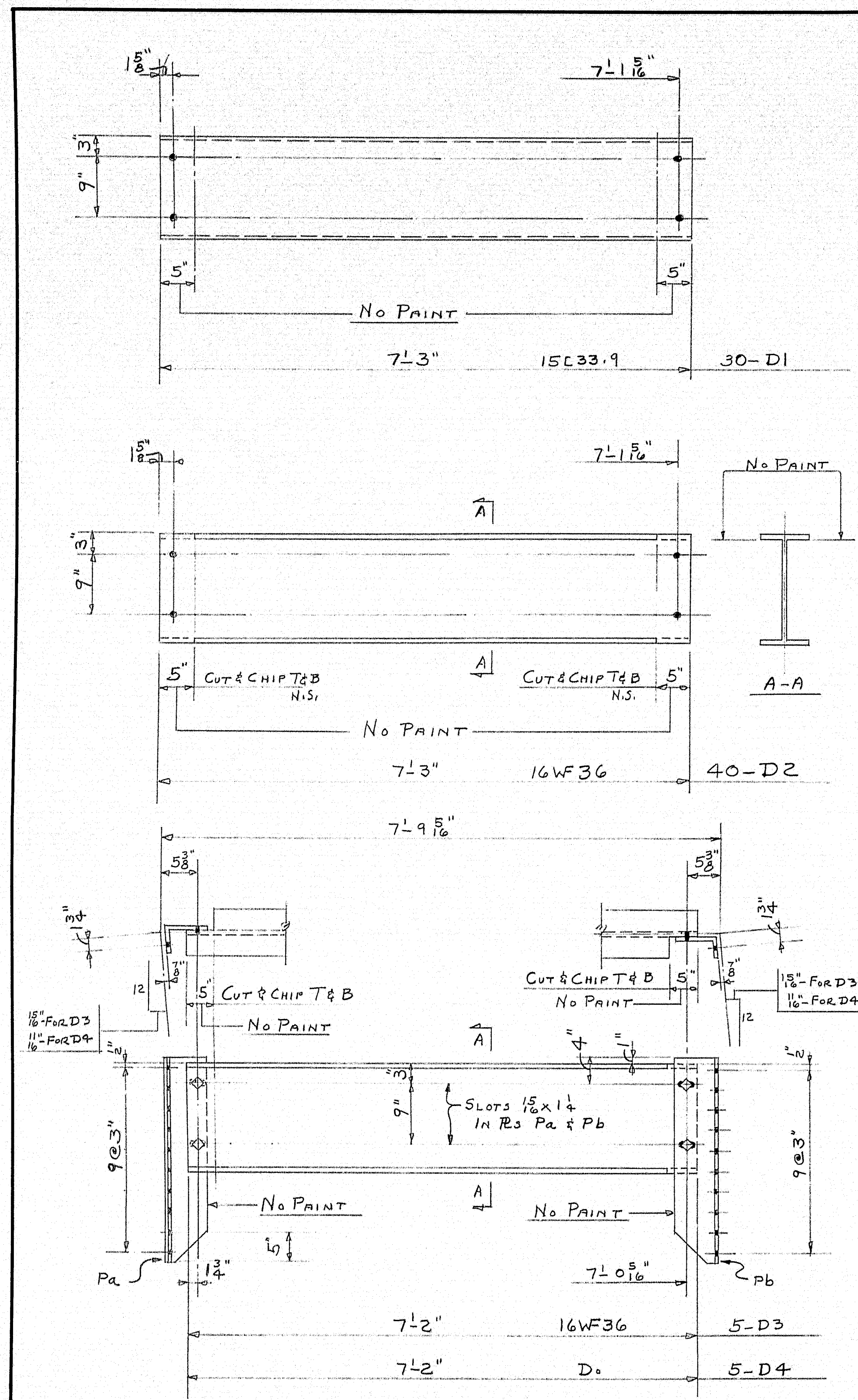


SHIP		BILL OF MATERIAL				DWG. B63-188-50
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S1B	1		36WF150	82' 5 1/2"		A3C
S2B	1		D.	82' 5 1/2"		
S3B	1		D.	82' 5 1/2"		
S4B	1		D.	82' 5 1/2"		
S5B	1		D.	82' 5 1/2"		
S6B	1		D.	82' 5 1/2"		
	12	P11	R 9 x 3/4	17' 0"		
	12	P12	R 11 x 1/2	3' 0 1/2"		
	24	P13	R 4 x 1 3/8	3' 0 1/2"		
	12	P14	R 10 1/2 x 1/2	2' 6"		
	5	P4	R 6 x 3/8	2' 7"		
	5	P5	D.	2' 7"		
	20	P6	D.	2' 7"		

SHOP CONNECTIONS: WELD (E70-ELECTRODES)
FIELD CONNECTIONS: 3/8 H.S. BOLTS
HOLES: 1 5/16" Ø
PAINT: STATE OF MAINE SPEC'S.

App'd 12-10-63			STRINGERS SPAN B SOUTHBOUND	
PRINT ISSUE			<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i> INT. #95 OVER MCRR & RTE 152 PITTSFIELD MAINE CUSTOMER SEAWARD CONST. CO. DESIGNER STATE HIGHWAY COMM.	
5	SHOP	12-20-63		
2	SHC	12-20-63		
3	CUST	12-20-63		
2	F/A	12-6-63	ORDER VERBAL DWG. B63-188-S6S SEGMENT (1)	
DRAWN	9-4-63	D.C.		
REVISION				
REVISION				
REVISION				





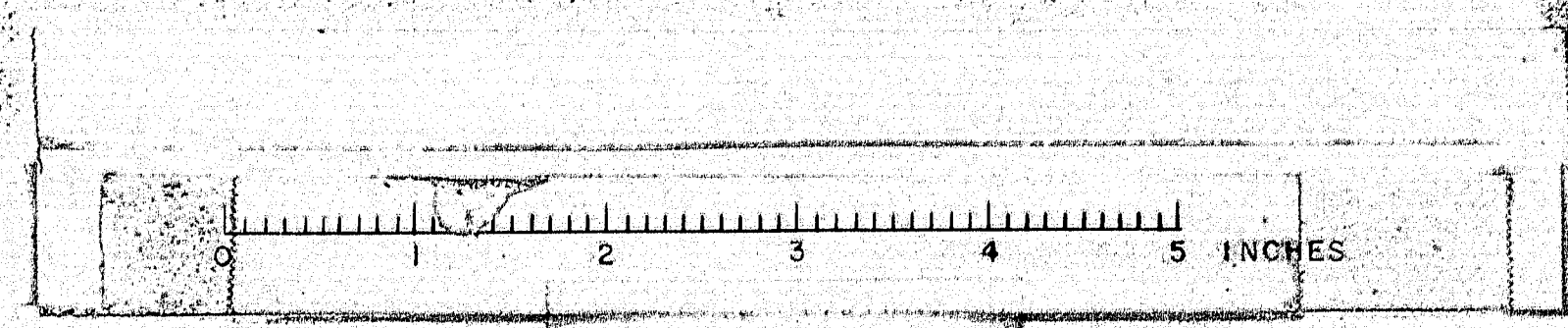
SHIP		BILL OF MATERIAL				DWG. B63-188-59
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
D1	30		15C33.9	7' 3"		A7
D2	40		16WF36	7' 3"		A7
D3	5		D.	7' 2"		A7
D4	5		D.	7' 2"		A7
		5	Pa3	2' 6"		BENT A34
		5	Pa4	2' 6"		D. A36
		5	Pb3	2' 6"		D. A36
		5	Pb4	2' 6"		D. A36
		40	SHOP	3/4" M. BOLT	0 1 1/2	

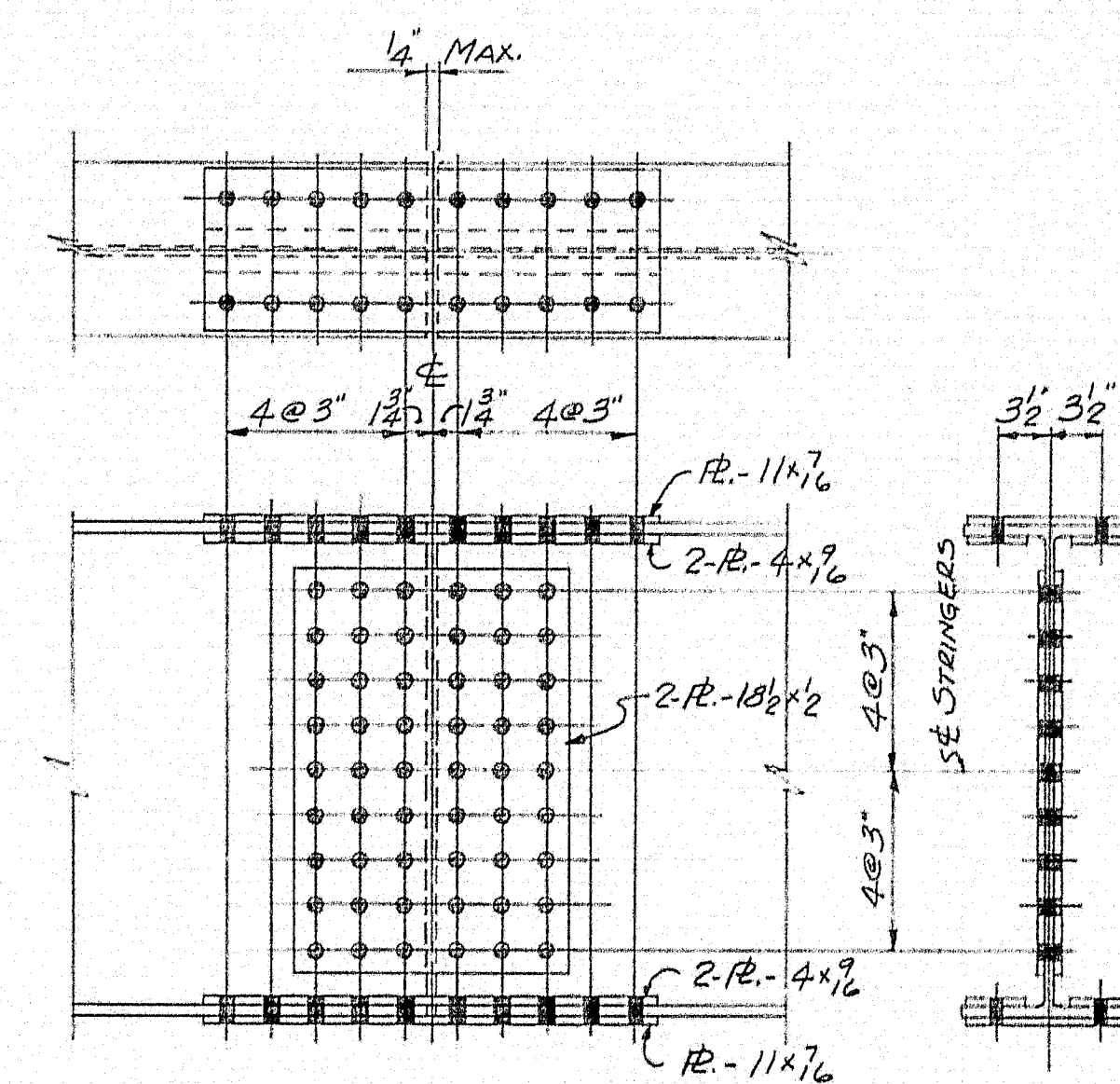
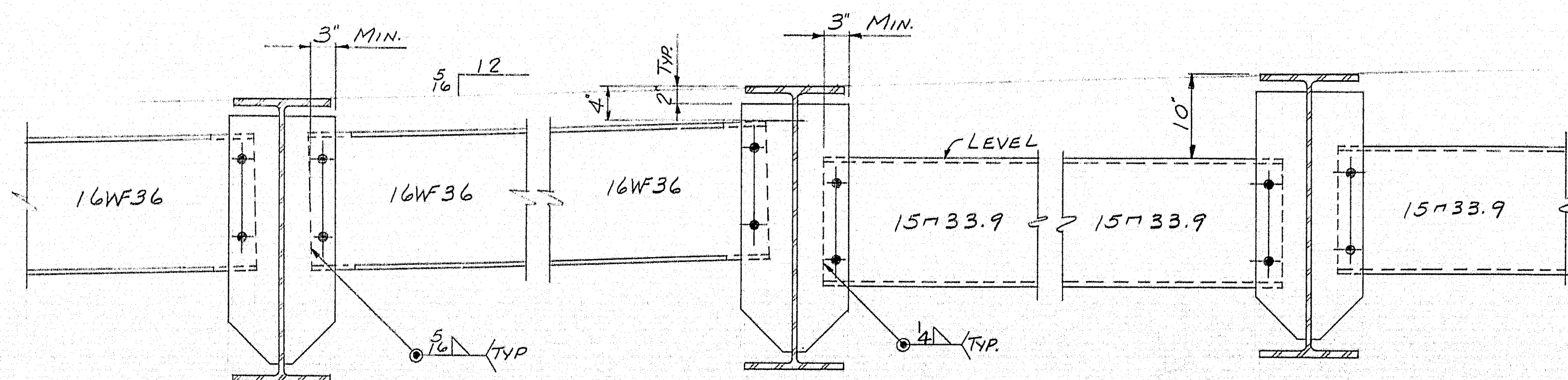
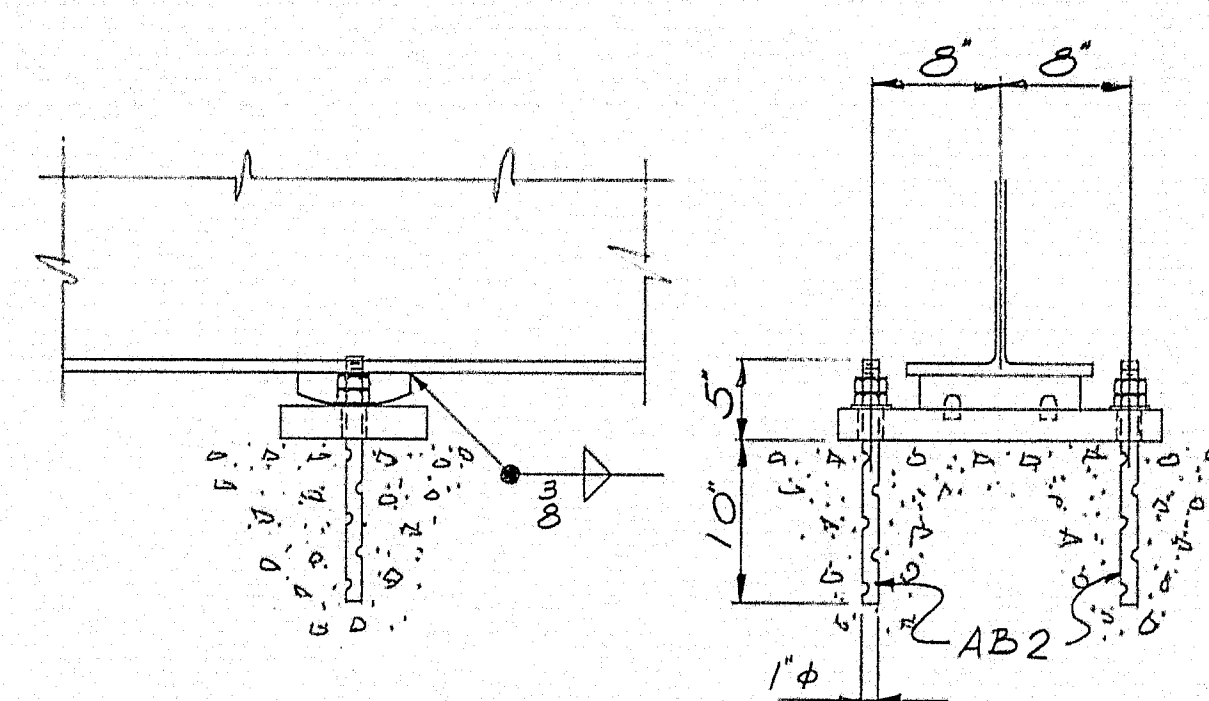
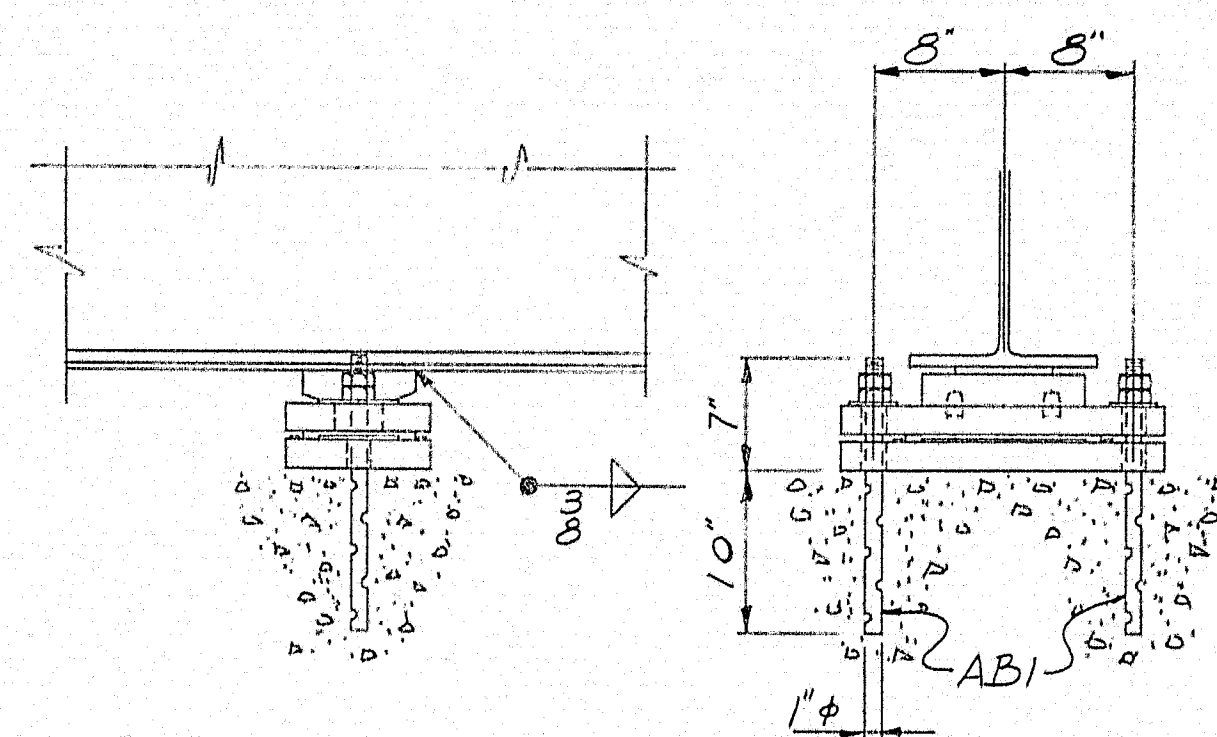
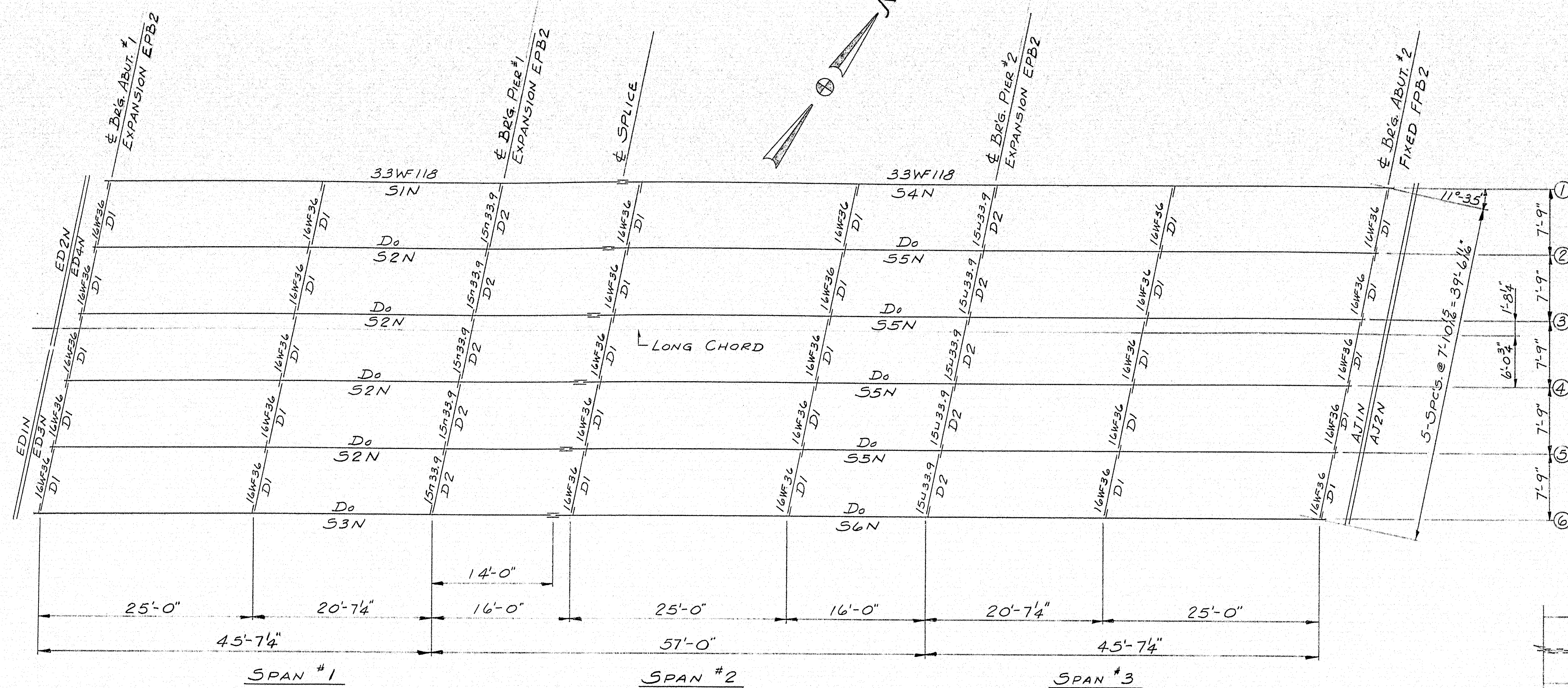
SHOP CONNECTIONS: 3/4" M. BOLT
 FIELD CONNECTIONS: 3/8" H.S. BOLTS & WELP
 HOLES: 1 5/8" Ø
 PAINT: STATE OF MAINE SPEC'S,

APP'D 12-10-63

DIAPHRAGMS		SOUTHBOUND	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
5	SHOP	12-20-63	INT. #95 OVER MCRR & RTE. 152 PITTSFIELD MAINE
2	SHC	12-20-63	
3	CUT	12-20-63	
2	F/A	12-6-63	
DRAWN	9-6-63	D.C.	CUSTOMER SEAWARD CONST. CO.
REVISION			DESIGNER STATE HIGHWAY COMM.
REVISION			ORDER VERBAL
REVISION			DWG. B63-188-595

SEGMENT ①





NOTE:
FIELD SPLICE HOLES
IN STRINGERS & SPLICE
RS. SHALL BE SUBPUNCHED
OR SUBDRILLED $\frac{1}{16}$ " & &
REAMED TO $\frac{1}{16}$ " & WITH
BEAMS SHOP ASSEMBLED.

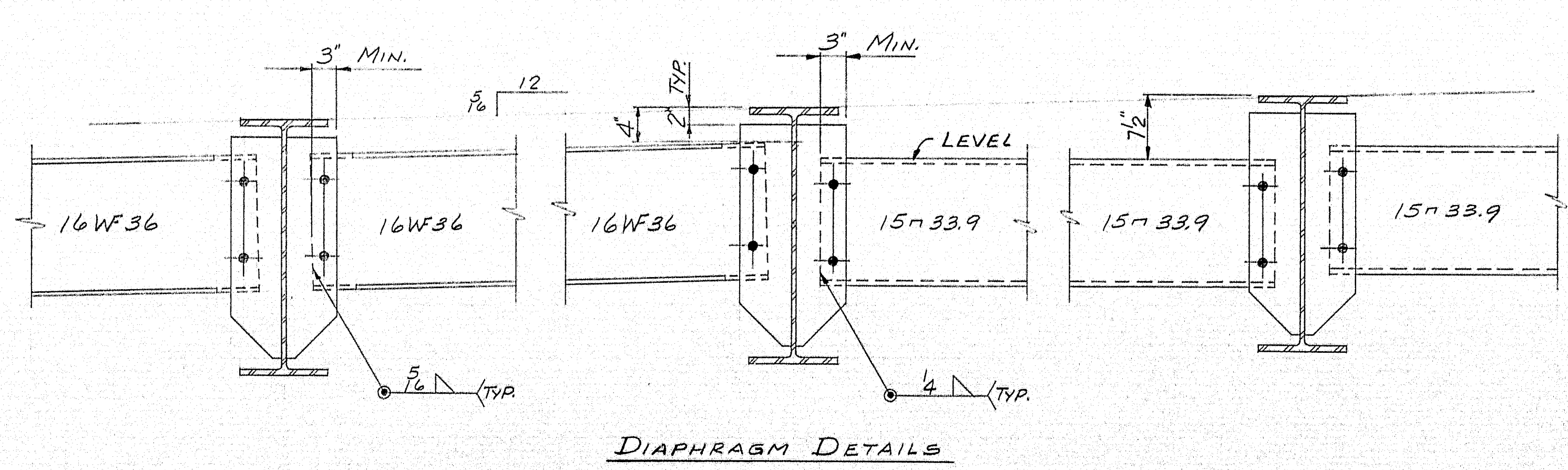
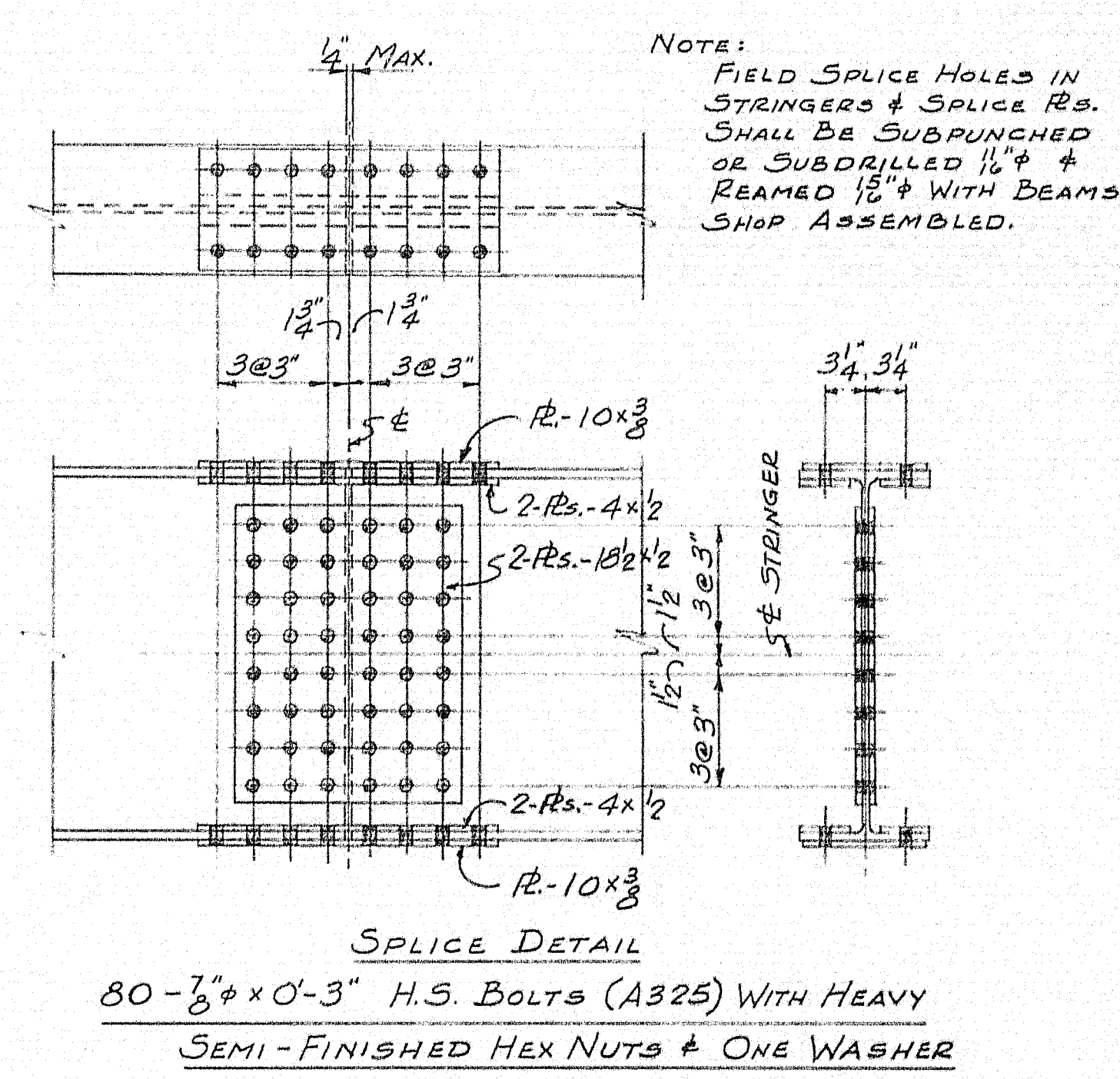
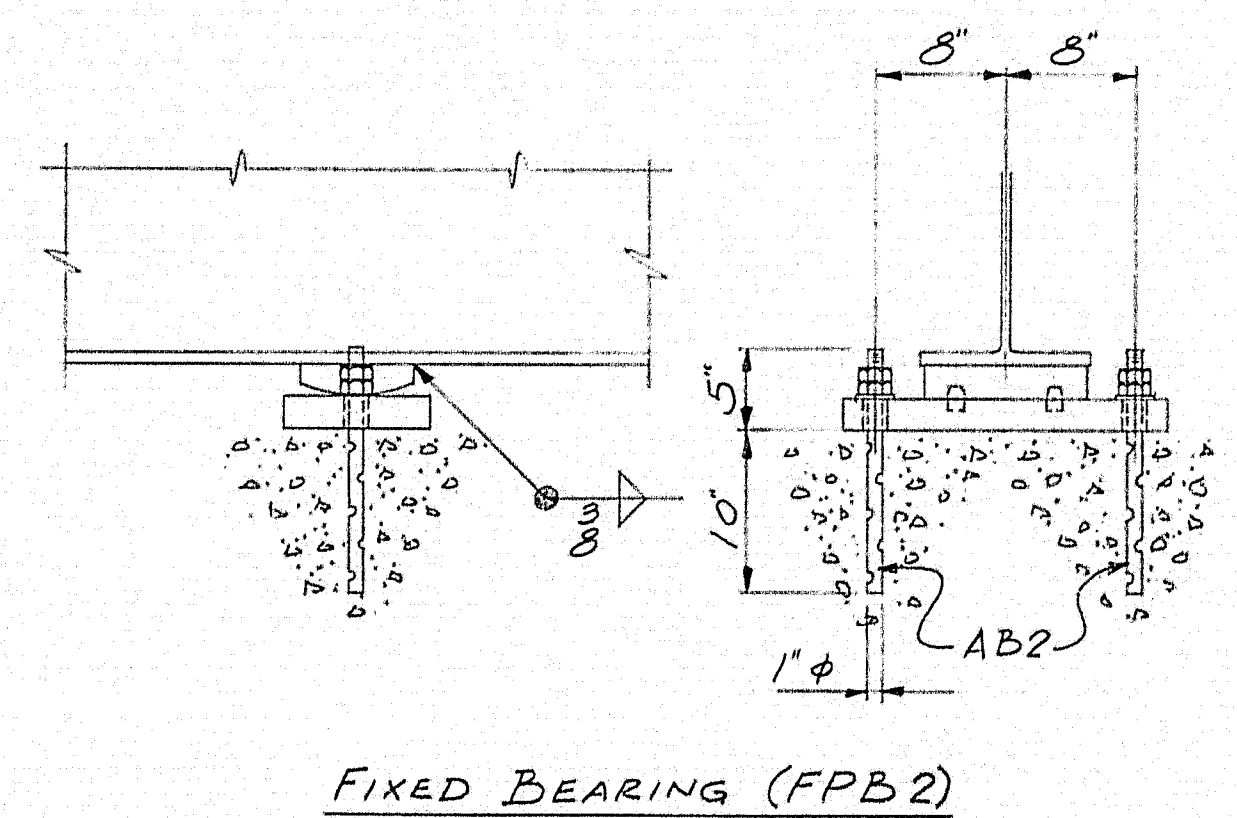
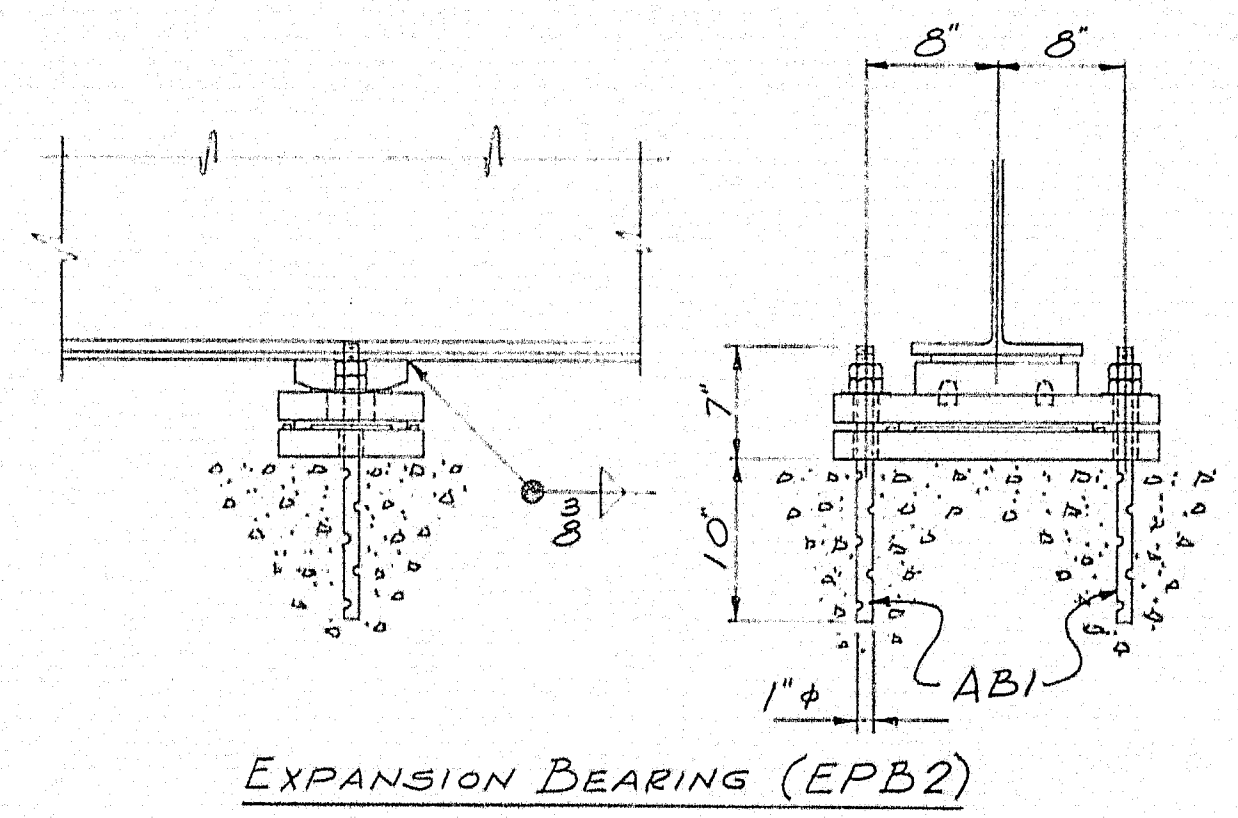
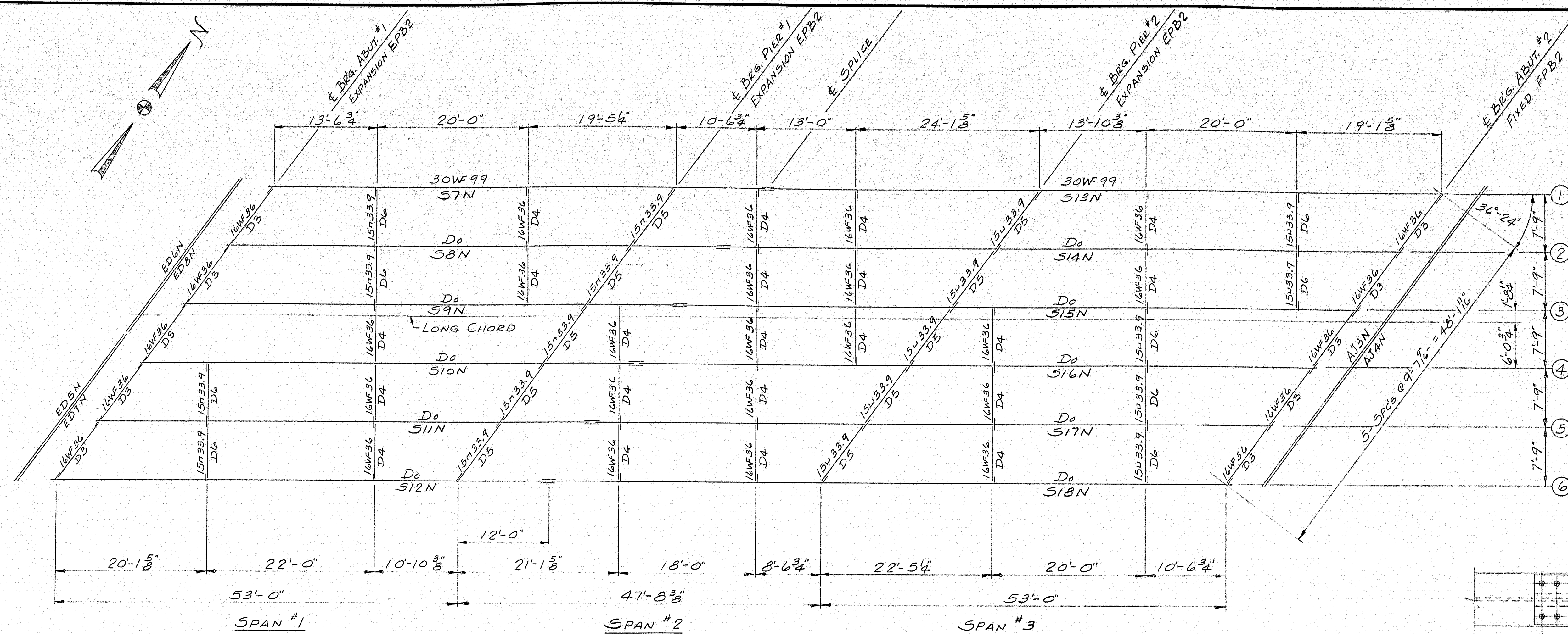
NOTE:
ALL STRUCTURAL STEEL SHALL CONFORM TO THE
REVISION OF THE SPECIFICATION A.S.T.M.
DESIGNATION A36 EXCEPT DIAPHRAGMS
WHICH MAY BE A36 OR A7.

SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS: WELD & H.S. BOLT
HOLES: AS NOTED
PAINT: STATE OF MAINE SPEC'S.

APP'D. AS NOTED 9-25-63

FRAMING PLAN - ROUTE #152 - NORTHBOUND

PRINT ISSUE			<i>Bancroft & Martin Rollings Mills Company</i> <i>Brewer, Maine</i>
2	SHC	10-24-63	INT. #95 OVER M.C.R.R. & ROUTE #152 PITTSFIELD, MAINE
3	CUST.	10-1-63	
1	SHOP	10-1-63	
2	F.A.	9-25-63	
2	F.A.	9-20-63	
DRAWN 9-12-63 2 A.M.			CUSTOMER <u>SEAWARD CONST.</u>
REVISION			DESIGNER <u>STATE HIGHWAY COMM.</u>
REVISION			
REVISION			ORDER <u>VERBAL</u> DWG. <u>B63-188-EIN</u>

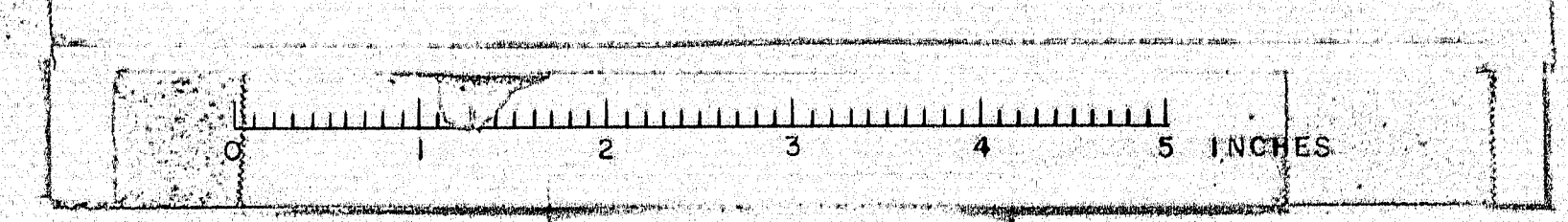


NOTE:
ALL STRUCTURAL STEEL SHALL CONFORM TO THE LATEST REVISION OF THE SPECIFICATION A.S.T.M. DESIGNATION A36 EXCEPT DIAPHRAGMS WHICH MAY BE A36 OR A7.

SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS: WELD & H.S. BOLT
HOLES: AS NOTED
PAINT: STATE OF MAINE SPEC'S.

APP'D. AS NOTED 9-25-63

FRAMING PLAN - M.C.R.R. - NORTHBOUND		
PRINT	ISSUE	
2 SHC	10-24-63	
3 CUST.	10-24-63	
1 SHOP	10-24-63	
2 F.A.	10-11-63	
2 F.A.	9-20-63	
DRAWN	9-18-63 R.A.M.	
REVISION		
REVISION		
REVISION		
ORDER VERBAL		DWG. B63-188-E2N



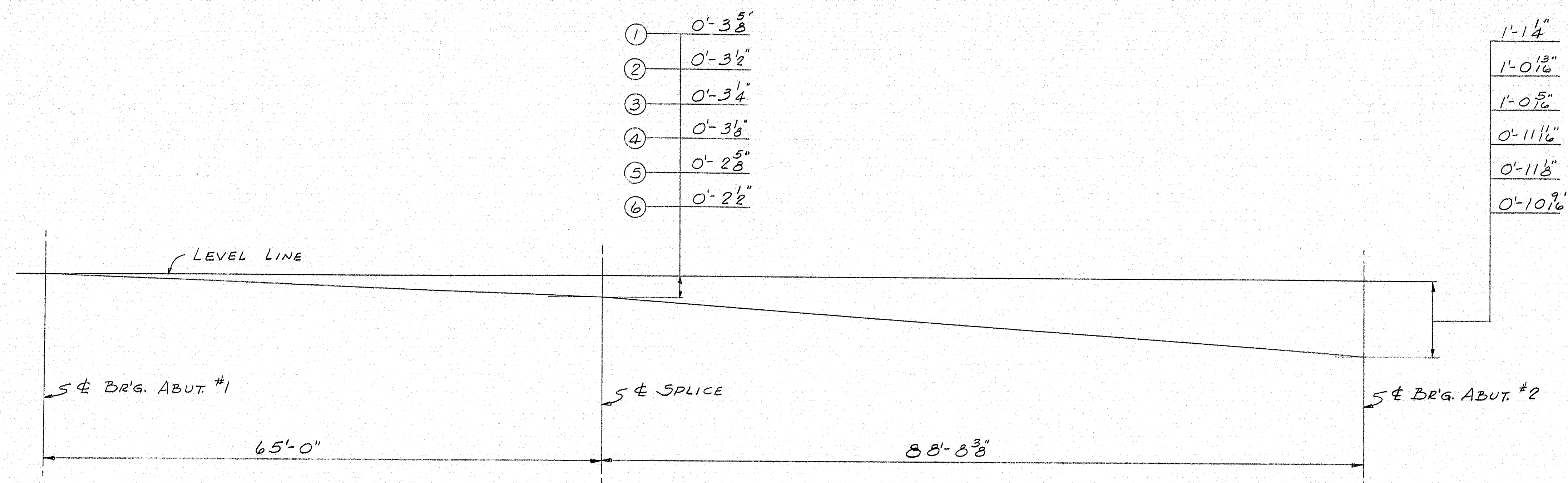


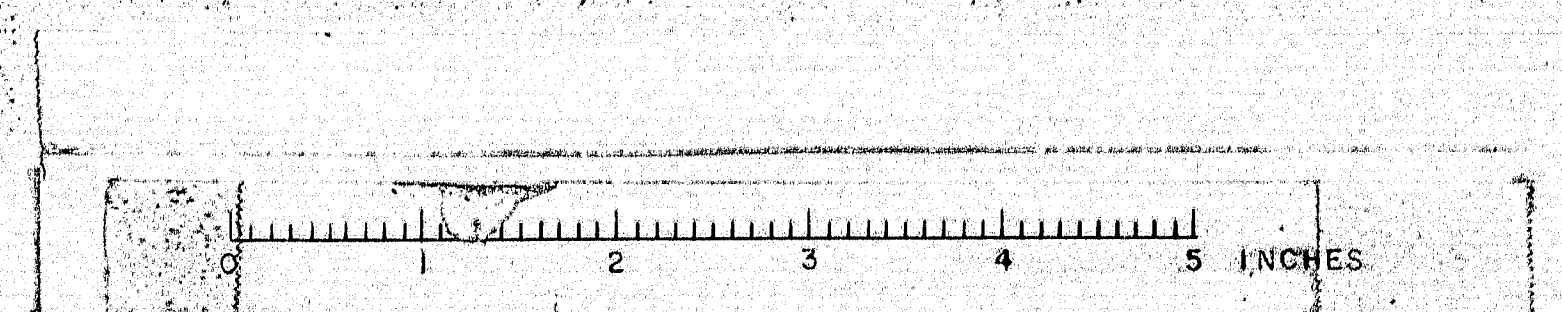
DIAGRAM OF STRINGER ELEVATIONS
COMPUTATIONS OF DIMENSIONS ARE BASED ON TOP OF STRINGER ELEVATIONS

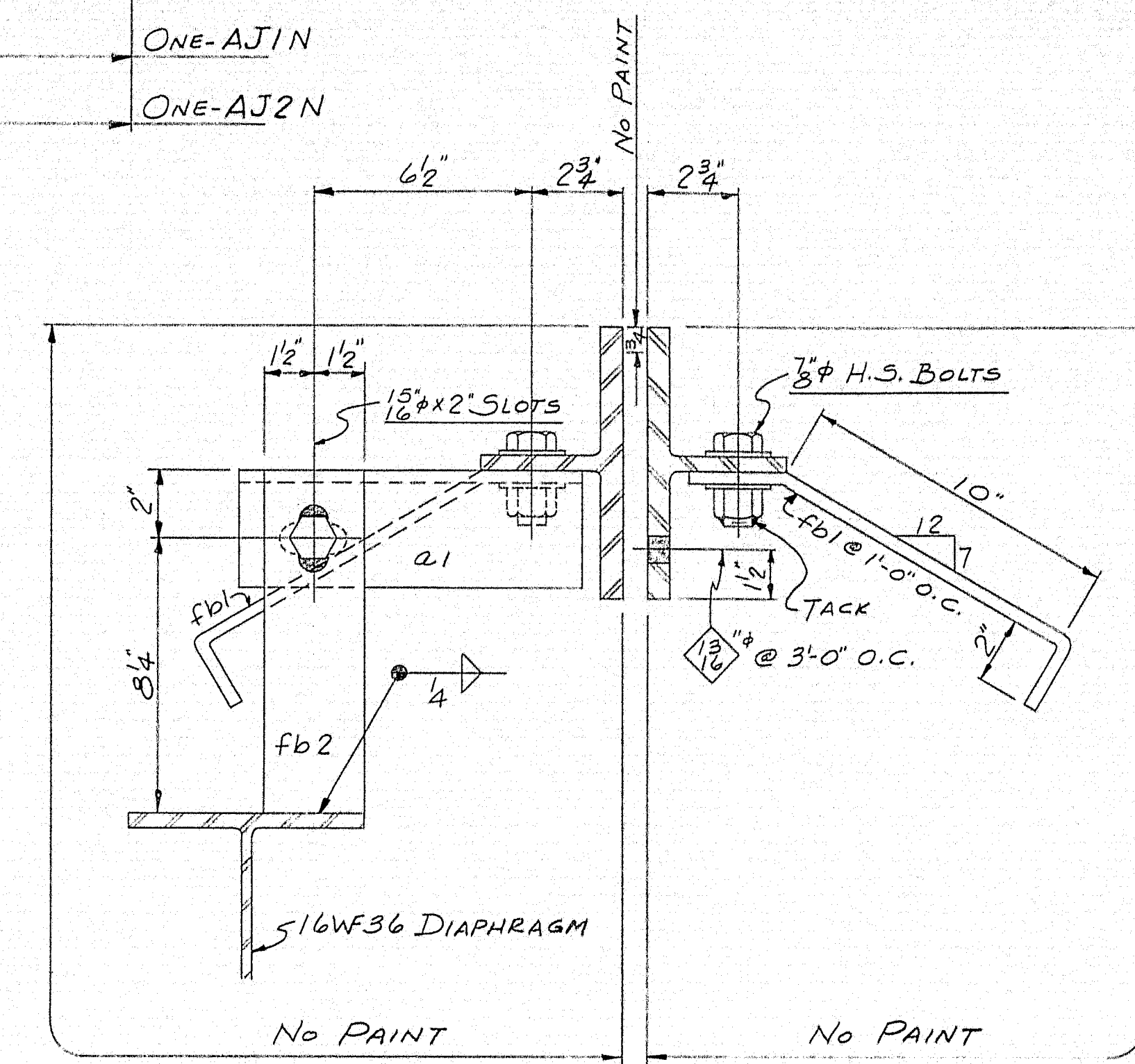
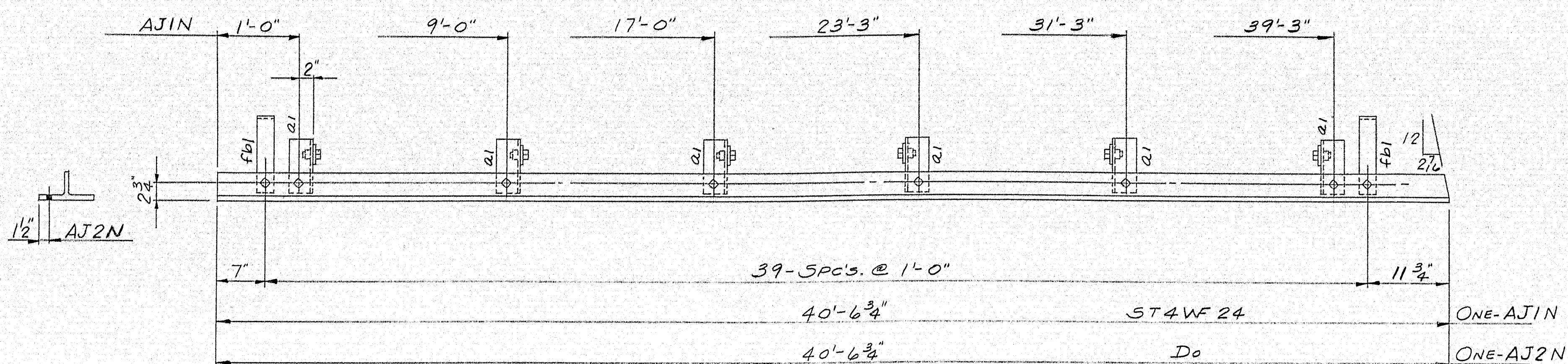
SHOP CONNECTIONS:
FIELD CONNECTIONS:
HOLES:
PAINT:

APP'D. AS NOTED 10-16-63

DIAGRAM OF STRINGER ELEVATIONS- M.C.R.R. - NORTHBOUND

PRINT ISSUE			Bancroft & Martin Rolling Mills Company Brewer, Maine	
NO.	DATE	BY		
2	SHC	10-24-63	INT. #95 OVER M.C.R.R. & RTE. #152 PITTSFIELD, MAINE	
3	CUST.	10-24-63		
1	SHOP	10-24-63	CUSTOMER SEAWARD CONST. DESIGNER STATE HIGHWAY COMM.	
2	F.A.	10-11-63		
DRAWN	10-1-63	R.A.M.	ORDER VERBAL	
REVISION				
REVISION			DWG. B63-188-E4N SEGMENT #1	
REVISION				



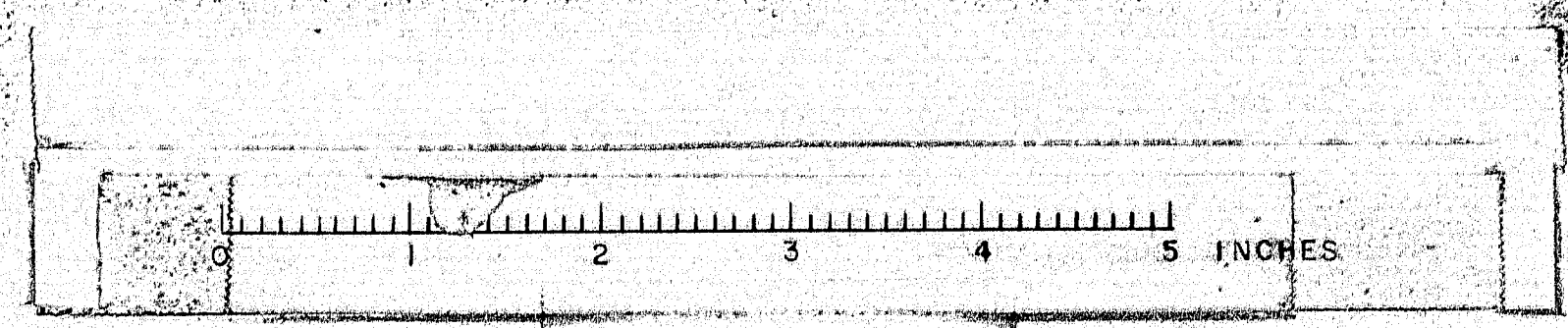


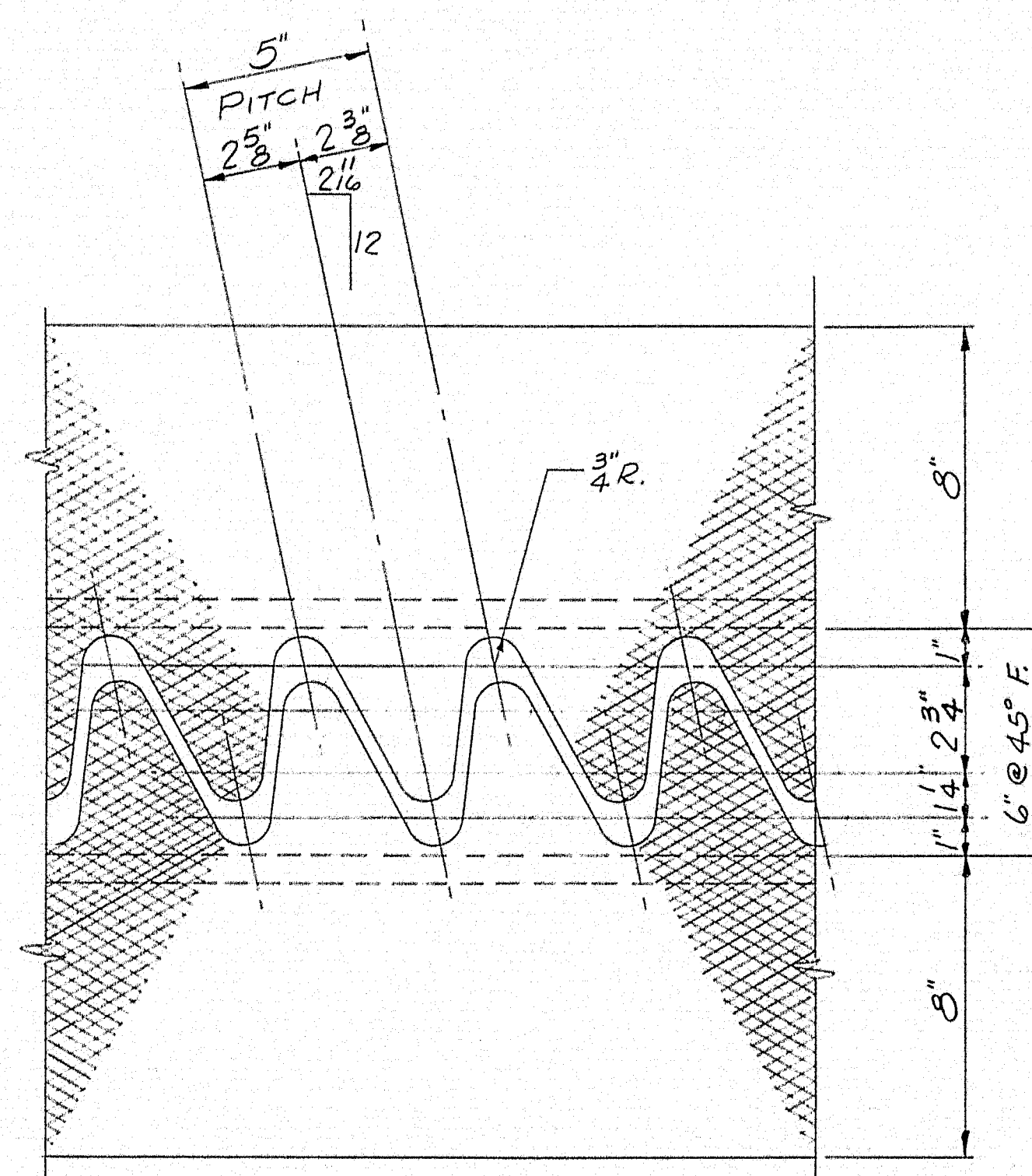
SHIP		BILL OF MATERIAL				DWG. B63-188-31N
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
AJIN	1		ST4WF24	40' 6 3/4"		A36
AJ2N	1		Do	40' 6 3/4"		
	6	a1	L-3 1/2 x 3 1/2 x 3/8	0' 10 1/4"		
	80	fb1	BAR-2 1/2 x 3/8	1' 3"		
	6	fb2	BAR-3 x 3/8	0' 10 1/4"		
	92	SHOP	3/8" H.S. BOLT	0' 2 1/4"		HEAVY NUTS A325
	184	DO	3/8" H.S. WASHER			

SHOP CONNECTIONS: 3/8" H.S. BOLT
 FIELD CONNECTIONS: WELD
 HOLES: 1/8" & U.N.
 PAINT: STATE OF MAINE SPECS.

APP'D. 9-25-63

PRINT ISSUE			ARMORED JOINTS - ROUTE #152 - NORTH BOUND	
			<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>	
2	SHC	10-24-63		
3	CUST	10-1-63	INT. #95 OVER M.C.R.R. & ROUTE #152 PITTSFIELD, MAINE	
5	SHOP	10-1-63		
2	F.A.	9-20-63	CUSTOMER SEAWARD CONST. DESIGNER STATE HIGHWAY COMM.	
	DRAWN	9-13-63 R.A.M.		
REVISION			ORDER VERBAL DWG. B63-188-31N SEGMENT #4	
REVISION				



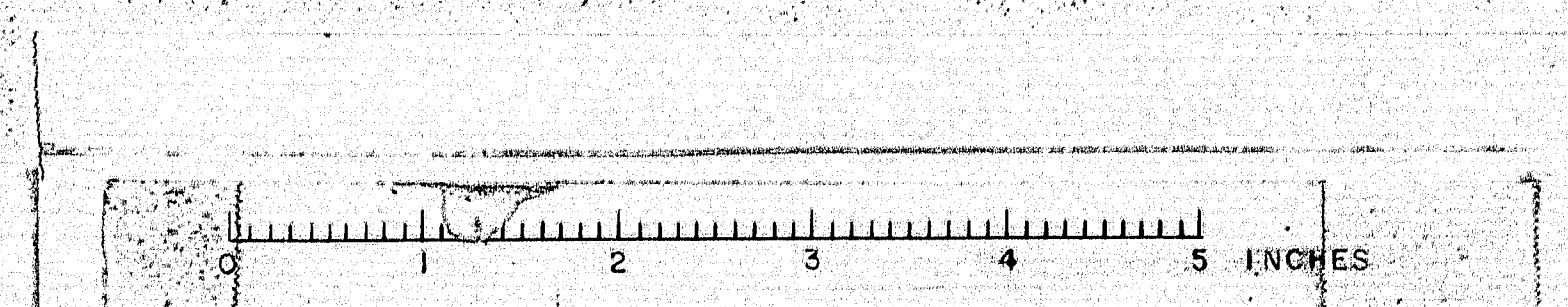
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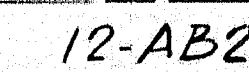
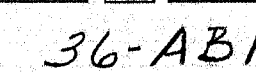
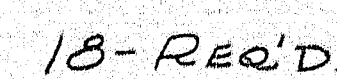
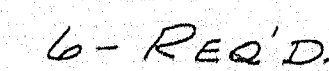
SECTION - "A" A'

SHOP CONNECTIONS: 7" ϕ H.S. BOLT & WELD (E70)
FIELD CONNECTIONS: WELD
HOLES: AS NOTED
PAINT: STATE OF MAINE SPEC'S.

EXPANSION DAMS - RTE. #152 - NORTHBOUND

PRINT ISSUE		<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>	
2	SHC	10-24-63	INT. #95 OVER M.C.R.R. & RTE. #152 PITTSFIELD, MAINE
3	CUST.	10-1-63	
5	SHOP	10-1-63	
2	FA.	9-20-63	
DRAWN	9-16-63 R.A.M.		CUSTOMER <u>SEAWARD CONST.</u>
REVISION			DESIGNER <u>STATE HIGHWAY COMM.</u>
REVISION			
REVISION			
ORDER <u>VERBAL</u>			DWG. <u>B63-188-32N</u> <u>SEGMENT #3</u>





SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS:
HOLES: AS NOTED
PAINT: STATE OF MAINE SPEC'S.

APP'D. AS NOTED 9-25-63

BEARING DETAILS - RTE. #152 - NORTHBOUND

Bancroft & Martin Inc.
Brewer, Maine

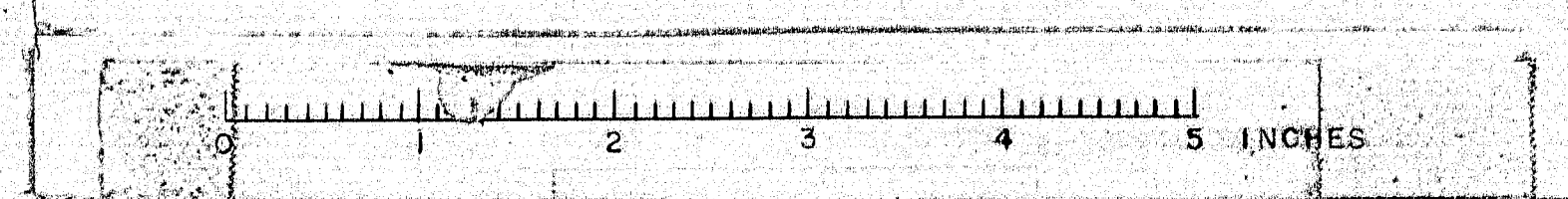
3 INT. #95 OVER M.C.R.R. & RTE. #152
PITTSFIELD MAINE

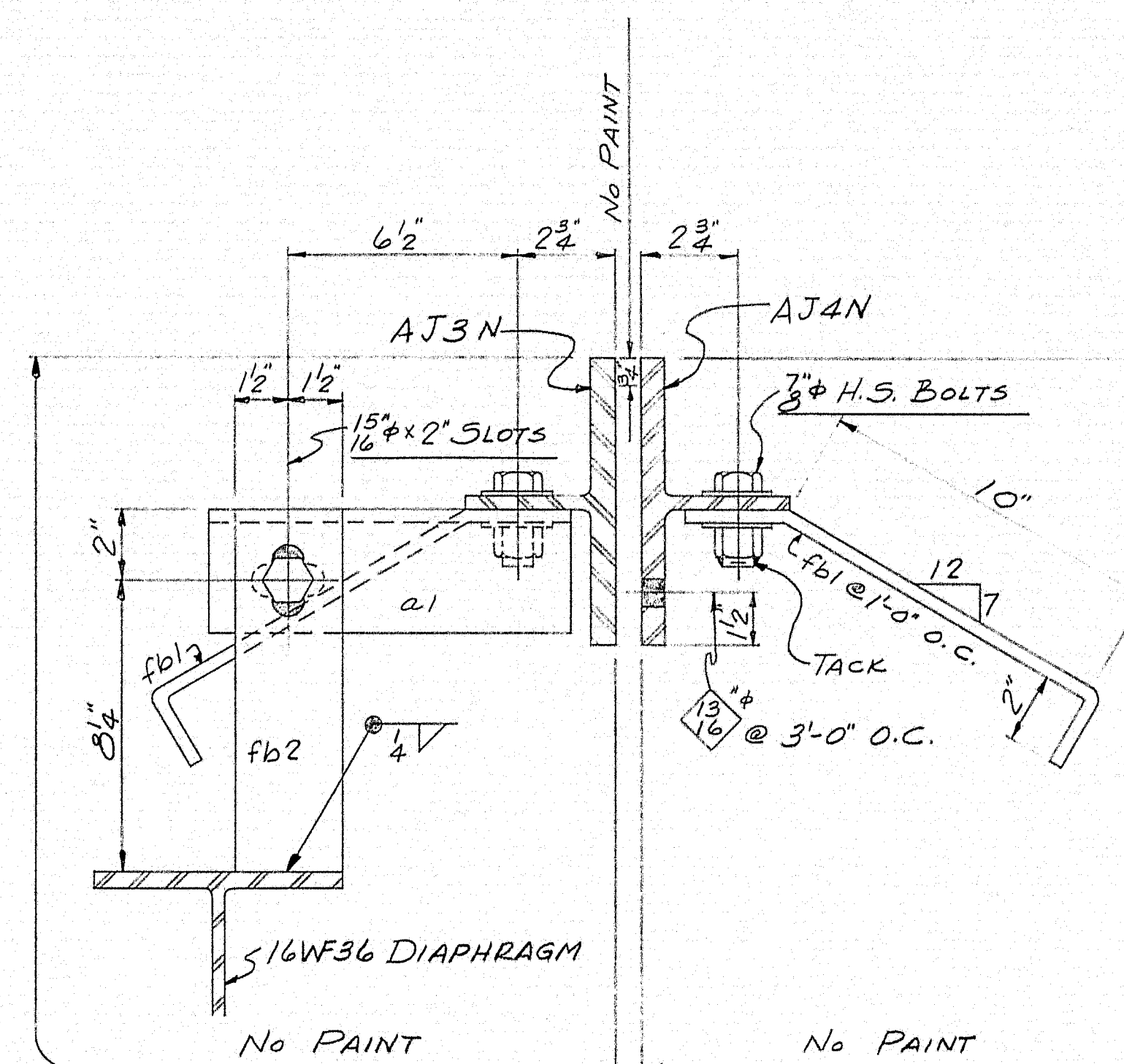
CUSTOMER SEAWARD CONST.
DESIGNER STATE HIGHWAY COMM.

ORDER <u>VERBAL</u>	DWG. <u>B63-188-53N</u> <u>SEGMENT #3</u>
---------------------	--

[illegible]

BEARING DETAILS - RTE. #152- NORTHBOUND				
PRINT ISSUE		<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>		
2	SHC	10-24-63	INT. #95 OVER M.C.R.R. # RTE. #152 PITTSFIELD, MAINE	
3	CUST	10-1-63		
2	PORT.	10-1-63		
2	F.A.	9-20-63		
DRAWN	9-17-63	R.A.M.	CUSTOMER <u>SEAWARD CONST</u>	
REVISION			DESIGNER <u>STATE HIGHWAY COMM.</u>	
REVISION			ORDER <u>VERBAL</u> DWG. <u>363-188-S3N</u> <u>SEGMENT # 3</u>	
REVISION				

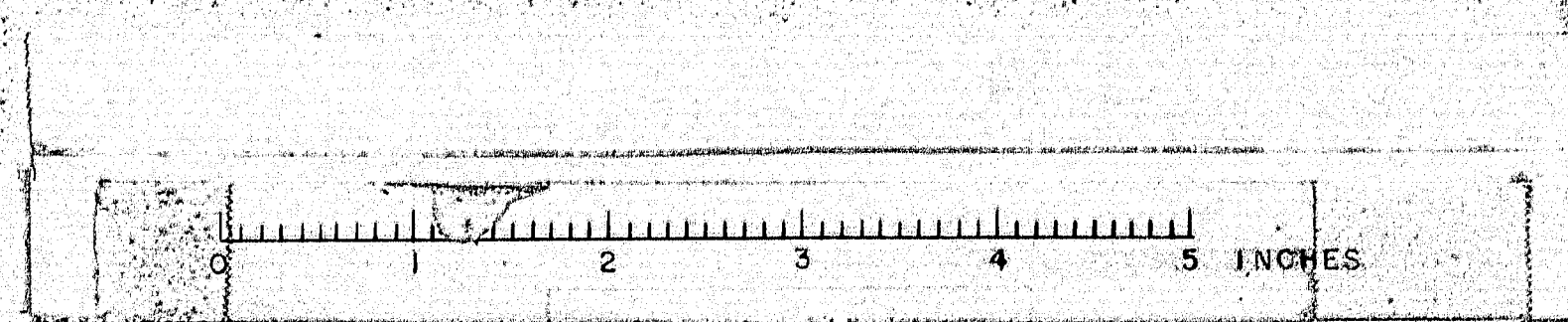


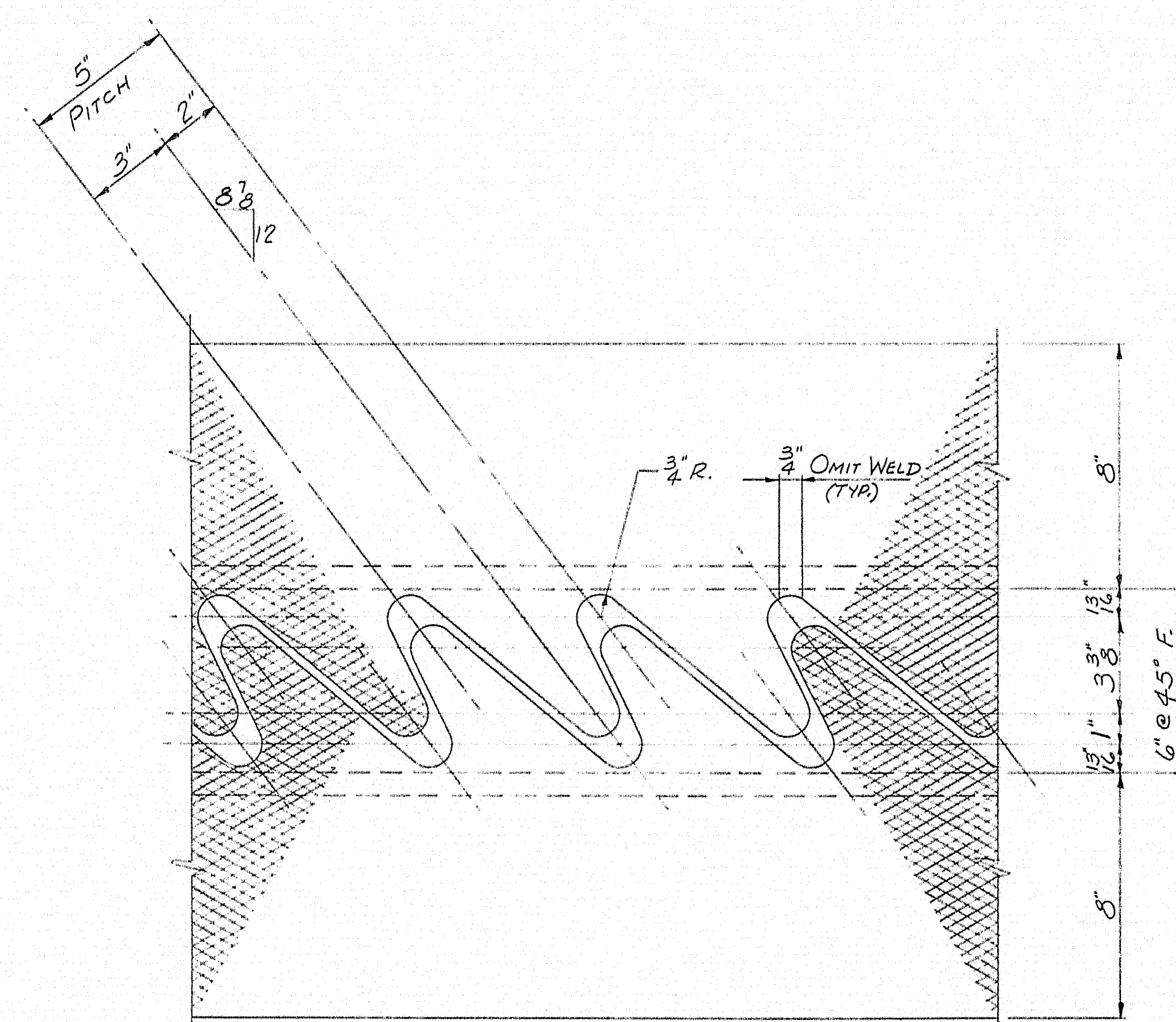
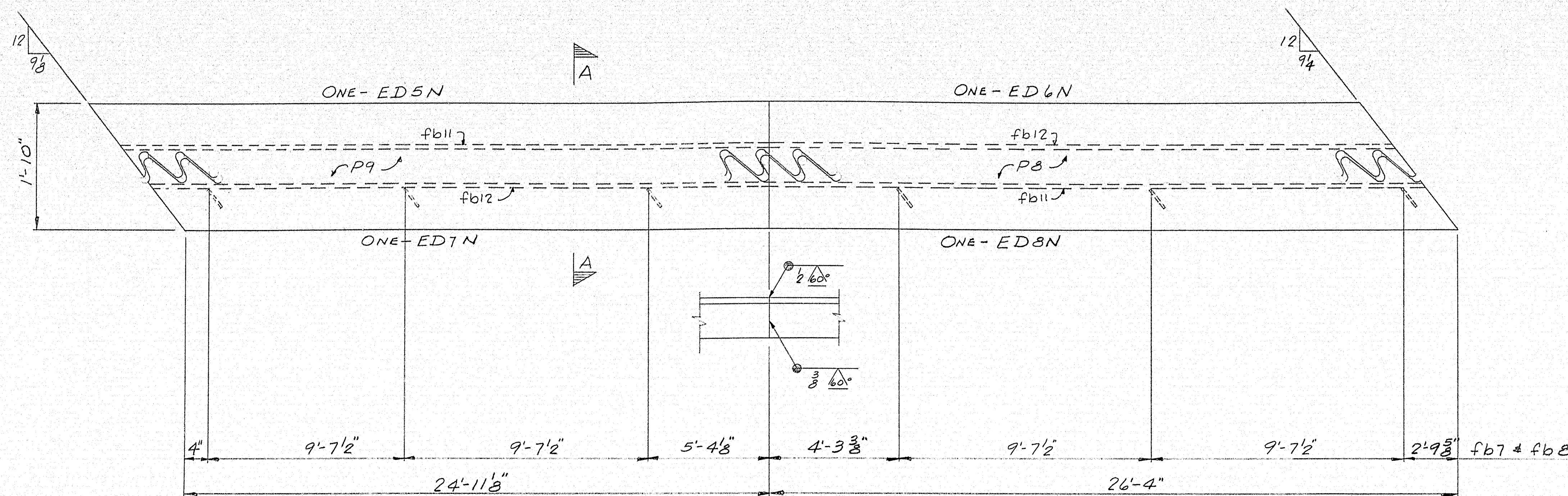


SHOP CONNECTIONS: $\frac{7}{8}$ " ϕ H.S. BOLT
FIELD CONNECTIONS: WELD
HOLES: $\frac{15}{16}$ " ϕ U.N.
PAINT: STATE OF MAINE SPEC'S.

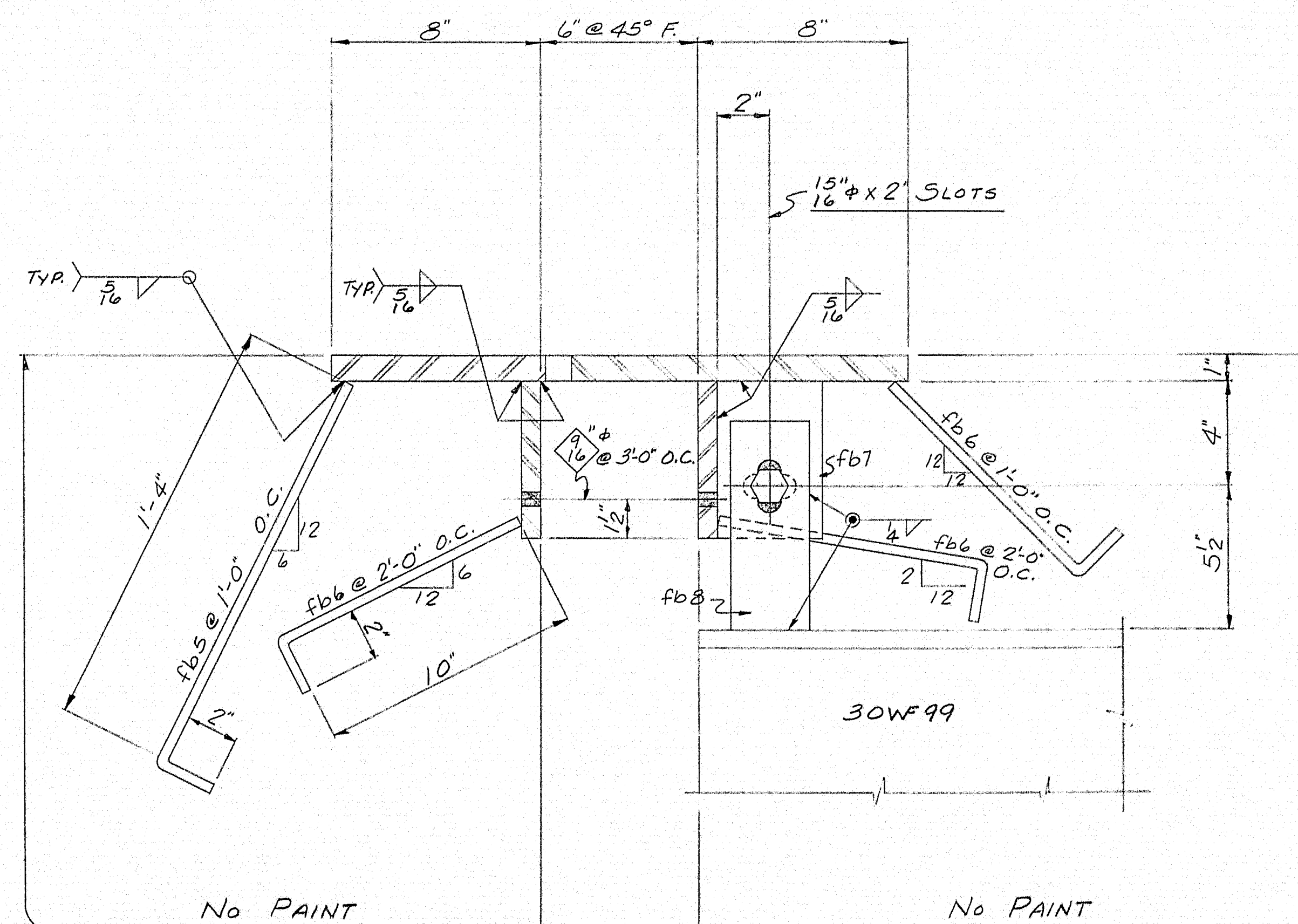
ARMORED JOINTS - M.C.R.R. - NORTHBOUND

PRINT ISSUE		<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>	
2	S.H.C. 10-24-63	INT. #95' OVER M.C.R.R. #RTE. #152 PITTSFIELD, MAINE	
3	CUST. 11-17-63		
5	SHOP. 10-17-63		
2	F.A. 9-20-63	CUSTOMER <u>SEAWARD CONST.</u> DESIGNER <u>STATE HIGHWAY COMM.</u>	
DRAWN	9-19-63 R.A.M.	ORDER <u>VERBAL</u>	DWG. <u>B-3-188-54N</u> SEGMENT #4
REVISION			
REVISION			
REVISION			





PARTIAL PLAN



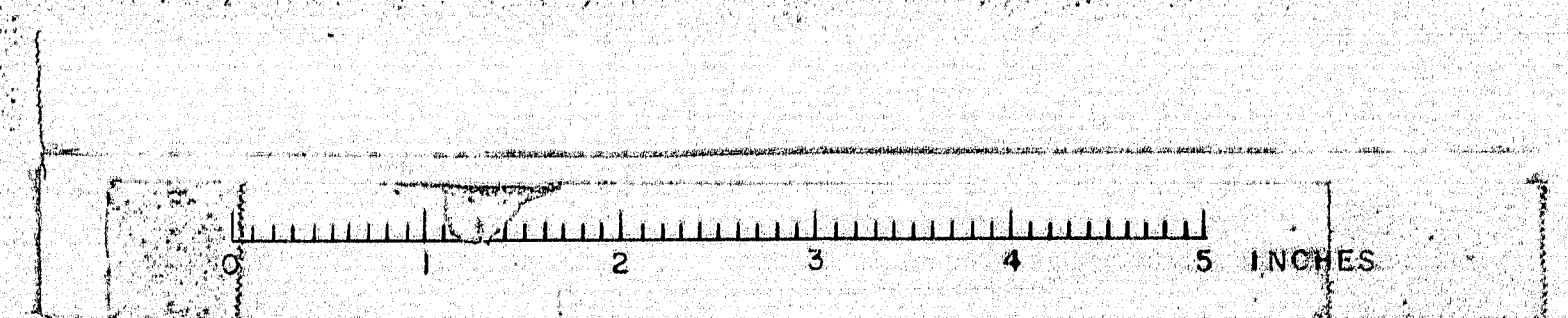
SECTION "AA"

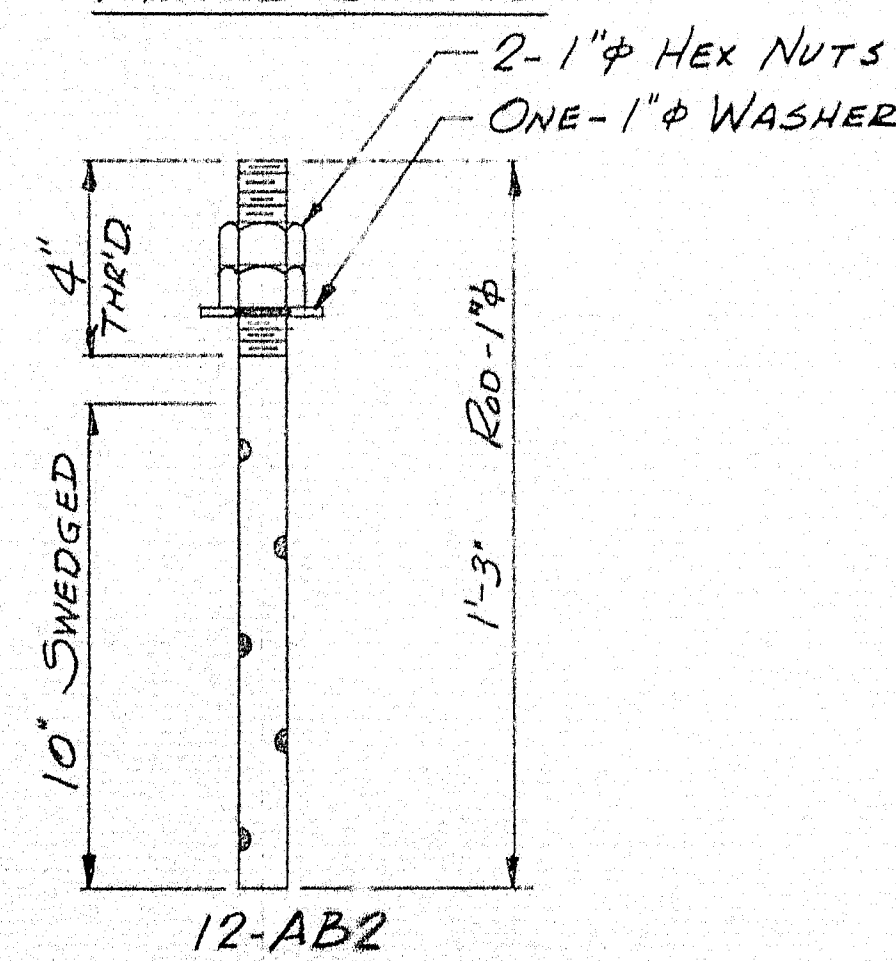
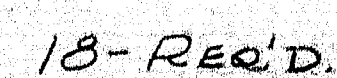
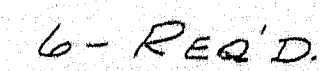
SHIP		BILL OF MATERIAL				DWG. B63-188-55N	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS	
ED5N	1		Exp. DAM.				
ED6N	1		D ₀				
ED7N	1		D ₀				
ED8N	1		D ₀				
	1	P8	R-21 x 1 x	26	4	Non-SKID	A36
	2	fb11	BAR-6 x $\frac{3}{4}$ x	25	10 $\frac{3}{8}$)
	2	fb12	D ₀	25	4 $\frac{3}{8}$		
	50	fb5	BAR-2 x $\frac{3}{8}$	1	6		
	100	fb6	D ₀	1	0		
	6	fb7	BAR-4 x $\frac{3}{8}$ x	0	6		
	6	fb8	BAR-3 x $\frac{3}{8}$ x	0	8)
	1	P9	R-21 x 1 x	26	3 $\frac{3}{8}$	Non-SKID	
	6	SHOP	$\frac{7}{8}$ " H.S. BOLT	0	2		A32
	6	D ₀	$\frac{7}{8}$ " H.S. WASHER				

SHOP CONNECTIONS: 7/8\"/>

APP'D AS NOTED 9-25-63

EXPANSION DAMS - M.C.R.R. - NORTHBOUND			
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
2	S.H.C.	10-24-63	INT. #95 OVER M.C.R.R. & RTE. #152
3	CVST.	11-17-63	PITTSFIELD, MAINE
5	SHOP	10-17-63	
2	F.A.	9-20-63	CUSTOMER SEAWARD CONST.
DRAWN 9-20-63 R.A.M.		DESIGNER STATE HIGHWAY COMM.	
REVISION			
REVISION			
REVISION			
		ORDER VERBAL	DWG. B63-188-55N SEGMENT 15



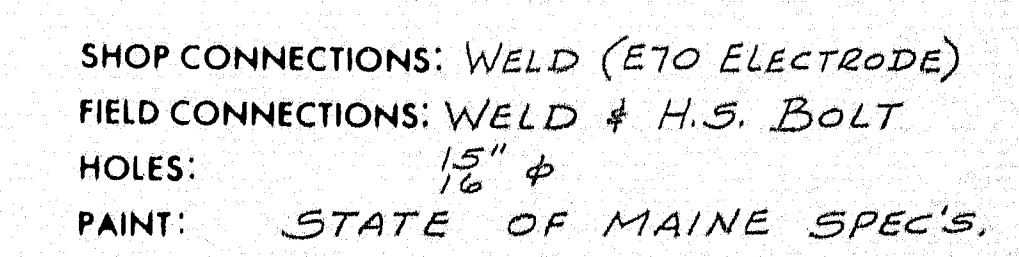
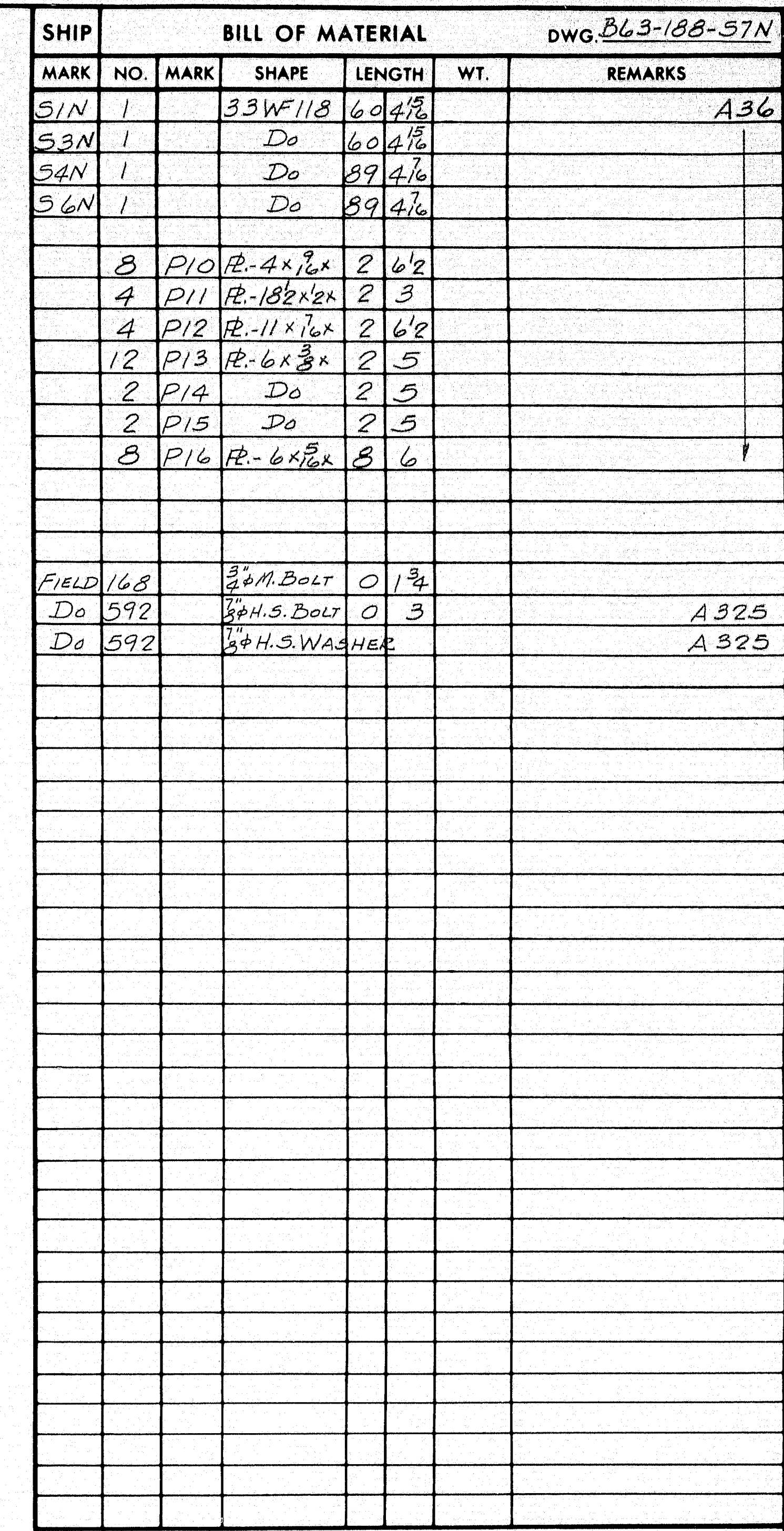


NO PAINT ON SURFACES FINISHED ASA125. COAT WITH HOT MIXTURE OF WHITE LEAD & TALLOW. NO PAINT ON TOP SURFACE & $\frac{3}{4}$ " DOWN FROM TOP SURFACE OF P3. ONE COAT OF BOILED LINSEED OIL. NO PAINT ON AB1 & AB2 OIL THREADS.

SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS:
HOLES: AS NOTED
PAINT: STATE OF MAINE SPEC'S.

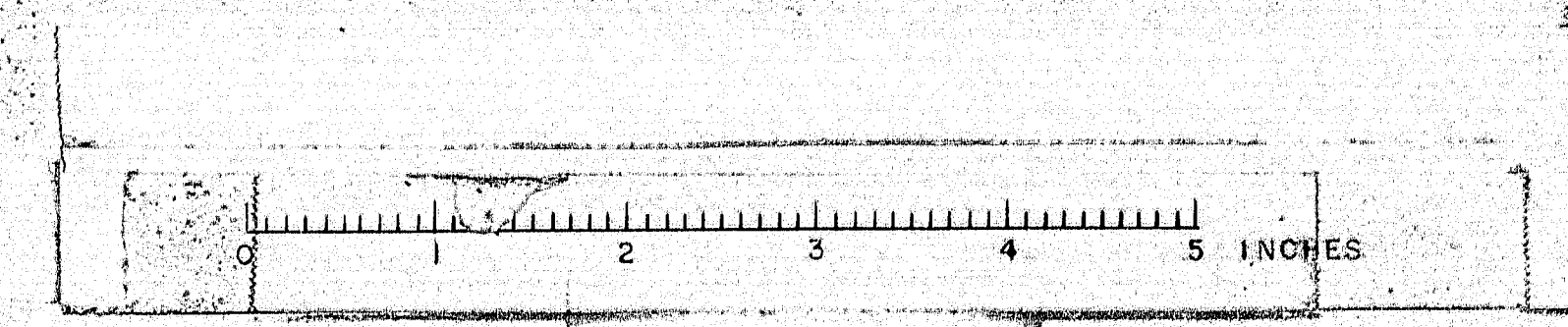
APP'D. AS NOTED 9-25-63

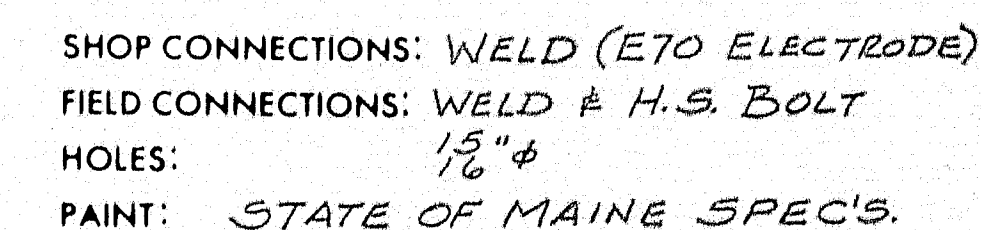
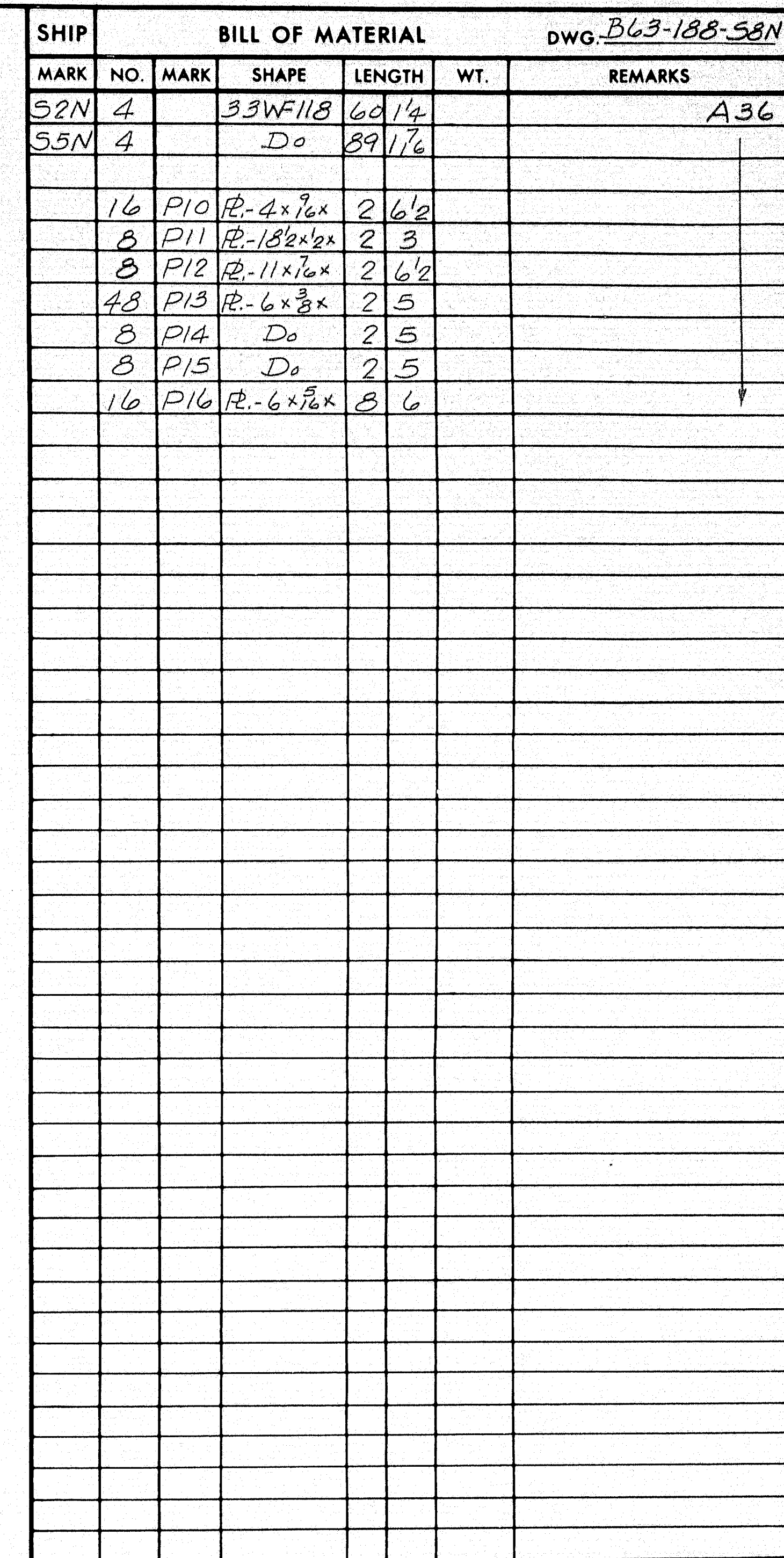
PRINT ISSUE			Bancroft & Martin Inc. Brewer, Maine	
2	S.H.C.	10-24-63	INT. #95 OVER M.C.R.R. # RTE. #152 PITTSFIELD MAINE	
3	CUST.	10-17-63		
2	P&T.	10-17-63		
1	S&OP	10-17-63		
2	F.A.	9-20-63		
DRAWN 9-20-63 R.A.M.			CUSTOMER SEAWARD CONST.	
REVISION			DESIGNER STATE HIGHWAY COMM.	
REVISION			ORDER VERBAL	
REVISION			DWG B43-188-56N SEGMENT #3	



APP'D. AS NOTED 9-30-63

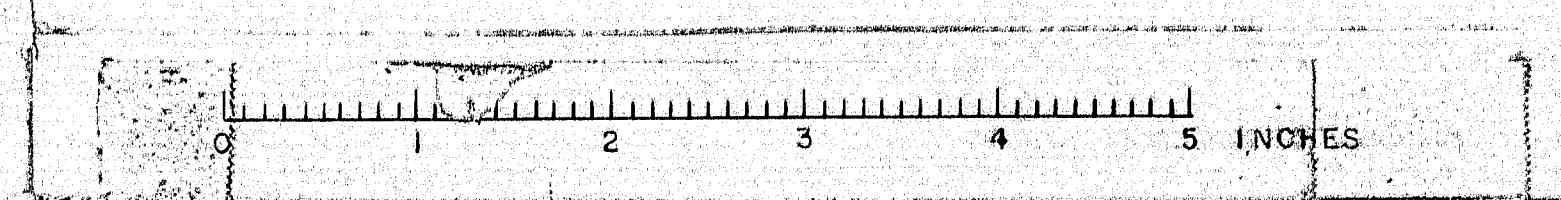
PRINT ISSUE			<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>	
2	SHC	10-24-63	INT. #95 OVER M.C.R.R. # RTE. #152 PITTSFIELD, MAINE	
3	CUST.	10-1-63		
5	SHOP	10-1-63		
2	F.A.	9-25-63		
DRAWN	9-23-63 R.A.M.		CUSTOMER <u>SEAWARD CONST.</u>	
REVISION			DESIGNER <u>STATE HIGHWAY COMM.</u>	
REVISION				
REVISION			ORDER <u>VERBAL</u>	DWG. <u>B63-188-57N</u> SEGMENT #1

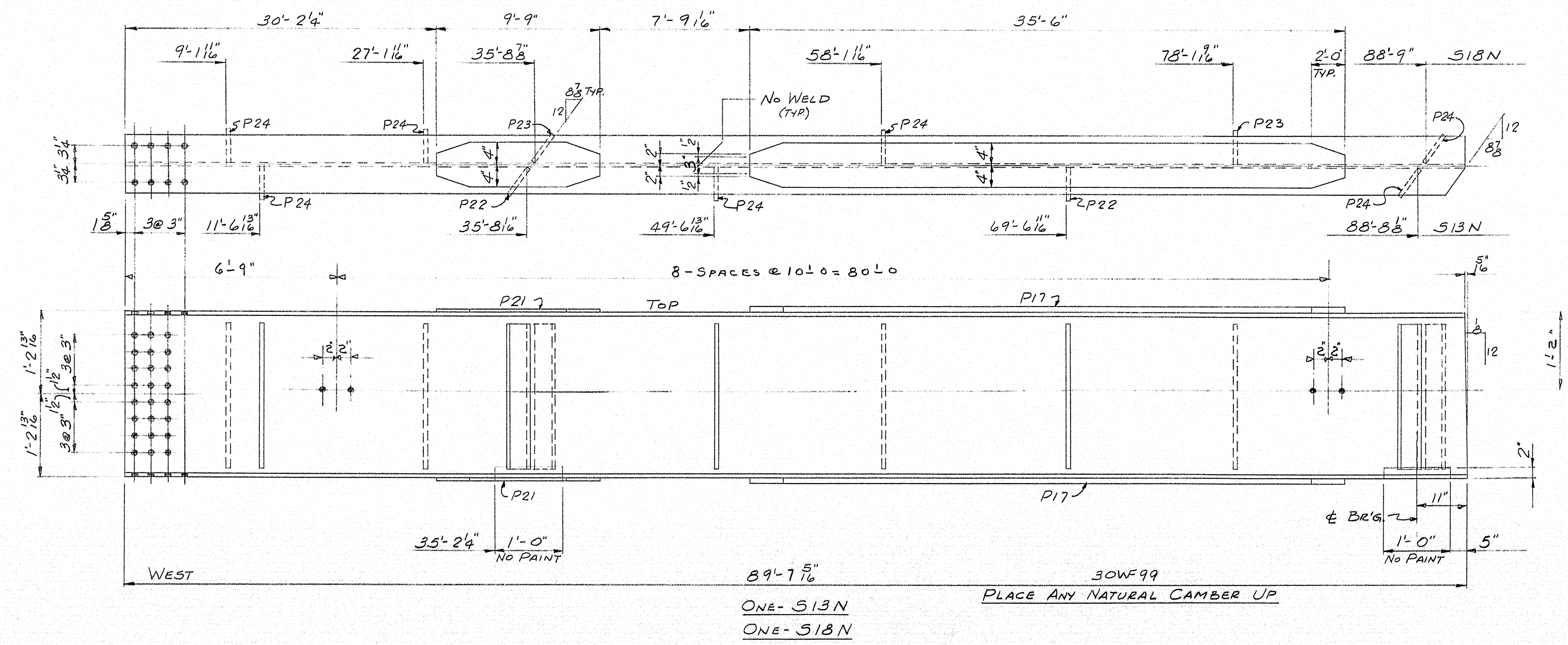
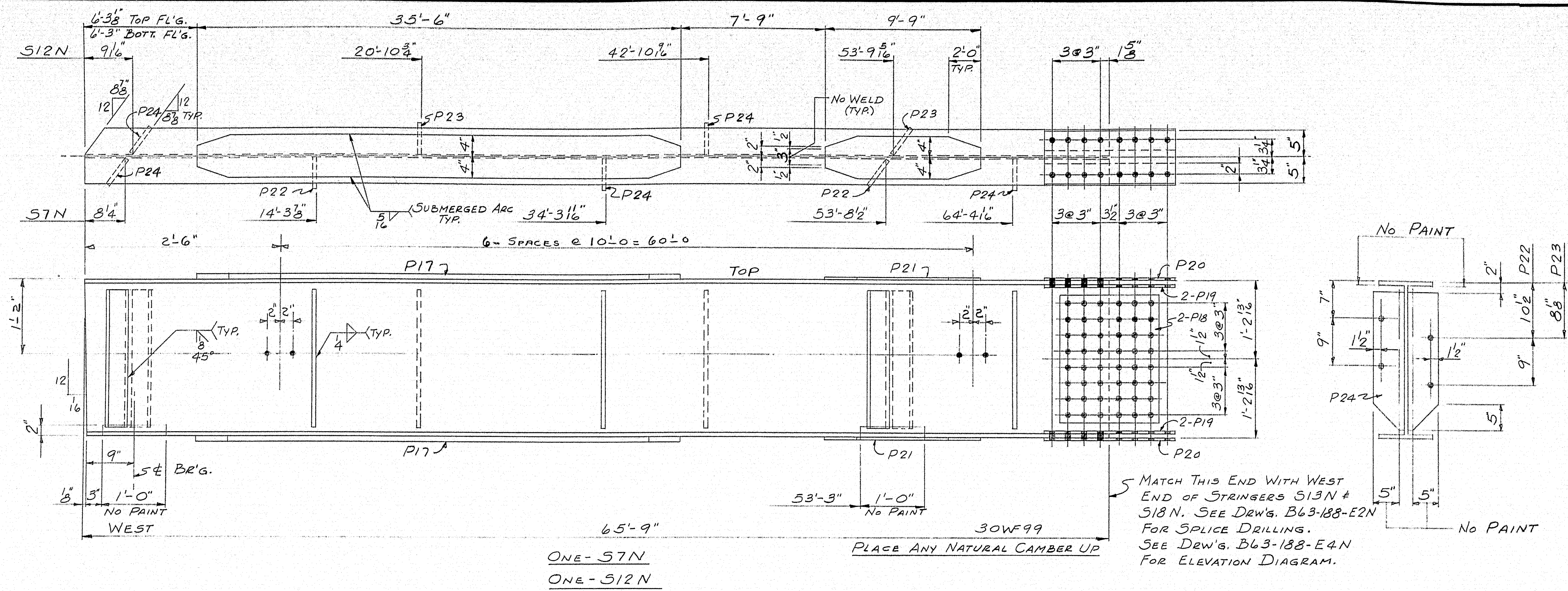




APP'D. AS NOTED 9-30-63

STRINGERS - RTE. #152 - NORTHBOUND			
PRINT ISSUE		<u>Bancroft & Martin Inc.</u> <u>Brewer, Maine</u>	
2	SHC	10-24-63	INT. #95 OVER M.C.R.R. & RTE. #152 PITTSFIELD, MAINE
3	CUST.	10-1-63	
5	SHOP	10-1-63	
2	F.A.	9-25-63	
DRAWN	9-24-63 R.A.M.		
REVISION			CUSTOMER <u>SEAWARD CONST.</u> DESIGNER <u>STATE HIGHWAY COMM.</u>
REVISION			
REVISION			
REVISION			
ORDER <u>VERBAL</u>		DWC <u>643-188-SLN</u> SEGMENT #1	

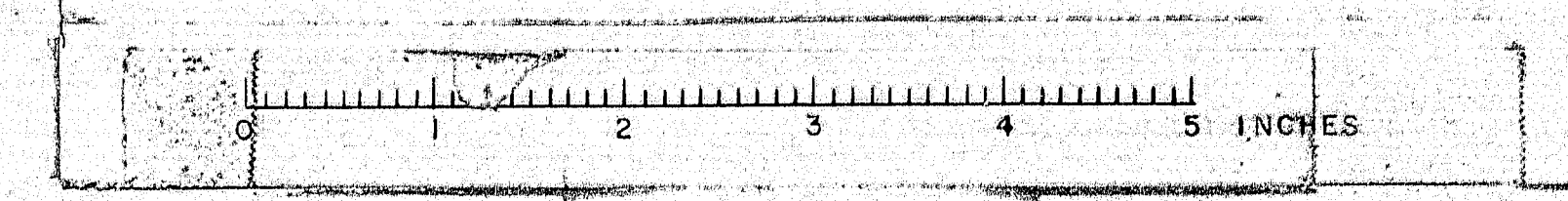


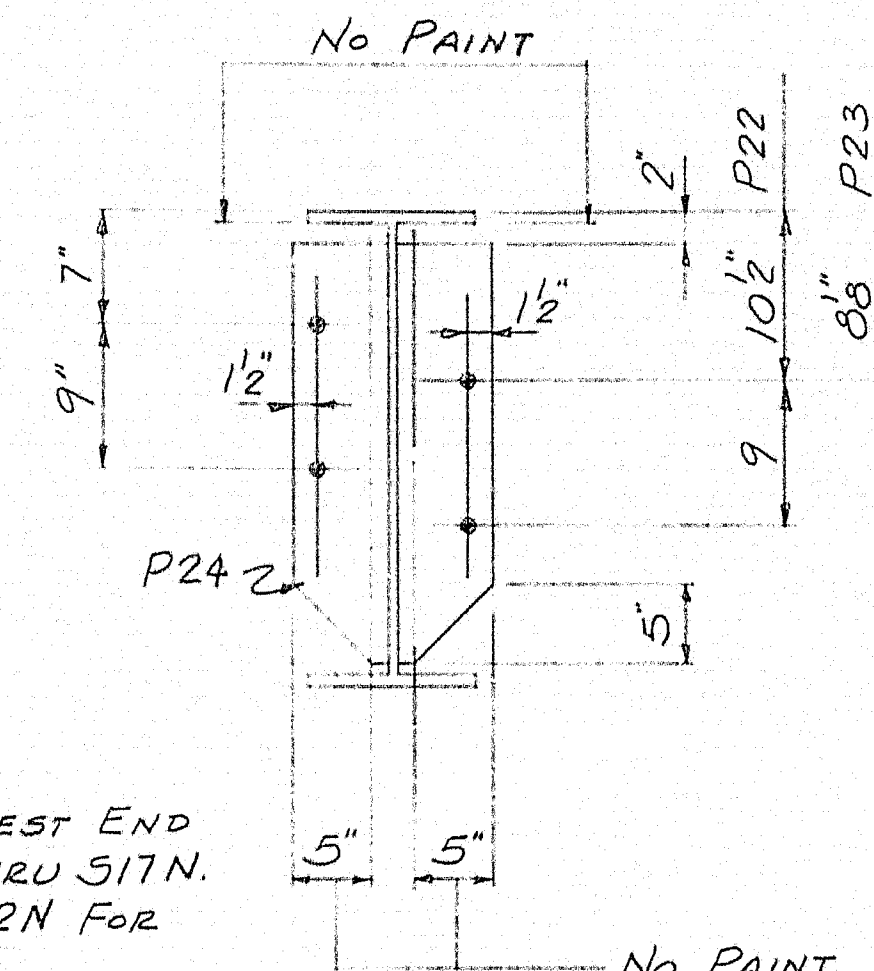


SHIP		BILL OF MATERIAL			DWG. B43-188-511N	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
57N	1		30WF99	65'9		A36
512N	1		Do	65'9		
513N	1		Do	89'7 $\frac{1}{2}$		
518N	1		Do	89'7 $\frac{1}{2}$		
	8	P17	R-8x1 $\frac{15}{16}$ x	35'6		
	4	P18	R-18 $\frac{1}{2}$ x2	2'0		
	8	P19	R-4x $\frac{1}{2}$	2'0 $\frac{1}{2}$		
12	P24	R-6x $\frac{3}{8}$	2'2			
4	P22	Do	2'2			
4	P23	Do	2'2			
4	P20	R-10x $\frac{3}{8}$	2'0 $\frac{1}{2}$			
8	P21	R-8x $\frac{3}{8}$	9'9			
FIELD	210		$\frac{3}{4}$ " M. BOLT	0'1 $\frac{3}{4}$		
Do	500		$\frac{3}{4}$ " H.S. BOLT	0'3		A325
Do	500		$\frac{3}{4}$ " H.S. WASHER			A325
	</					

SHOP CONNECTIONS: WELD (E70 ELECTRODE)
 FIELD CONNECTIONS: WELD & H.S. BOLT
 HOLES: 1/8" φ
 PAINT: STATE OF MAINE SPEC'S.

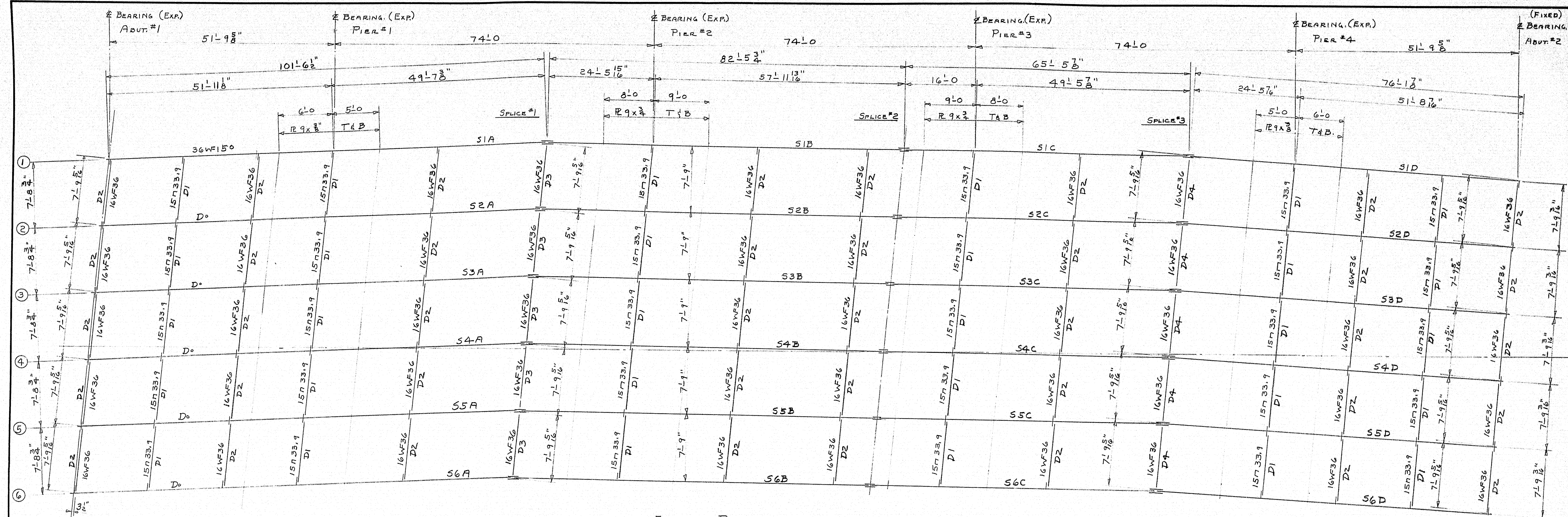
APP'D. 10-16-63		STRINGERS - M.C.R.R. - NORTHBOUND	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
2 SHC 10-24-63		INT. #95 OVER M.C.R.R. # RTE. #152	
3 CUST. 10-24-63		PITTSFIELD, MAINE	
5 SHOP 10-24-63		CUSTOMER SEAWARD CONST.	
2 F.A. 10-11-63		DESIGNER STATE HIGHWAY COMM.	
DRAWN 9-26-63 R.A.M.		ORDER VERBAL	
REVISION		DWG. B63-188-511N	
REVISION		SEGMENT #1	



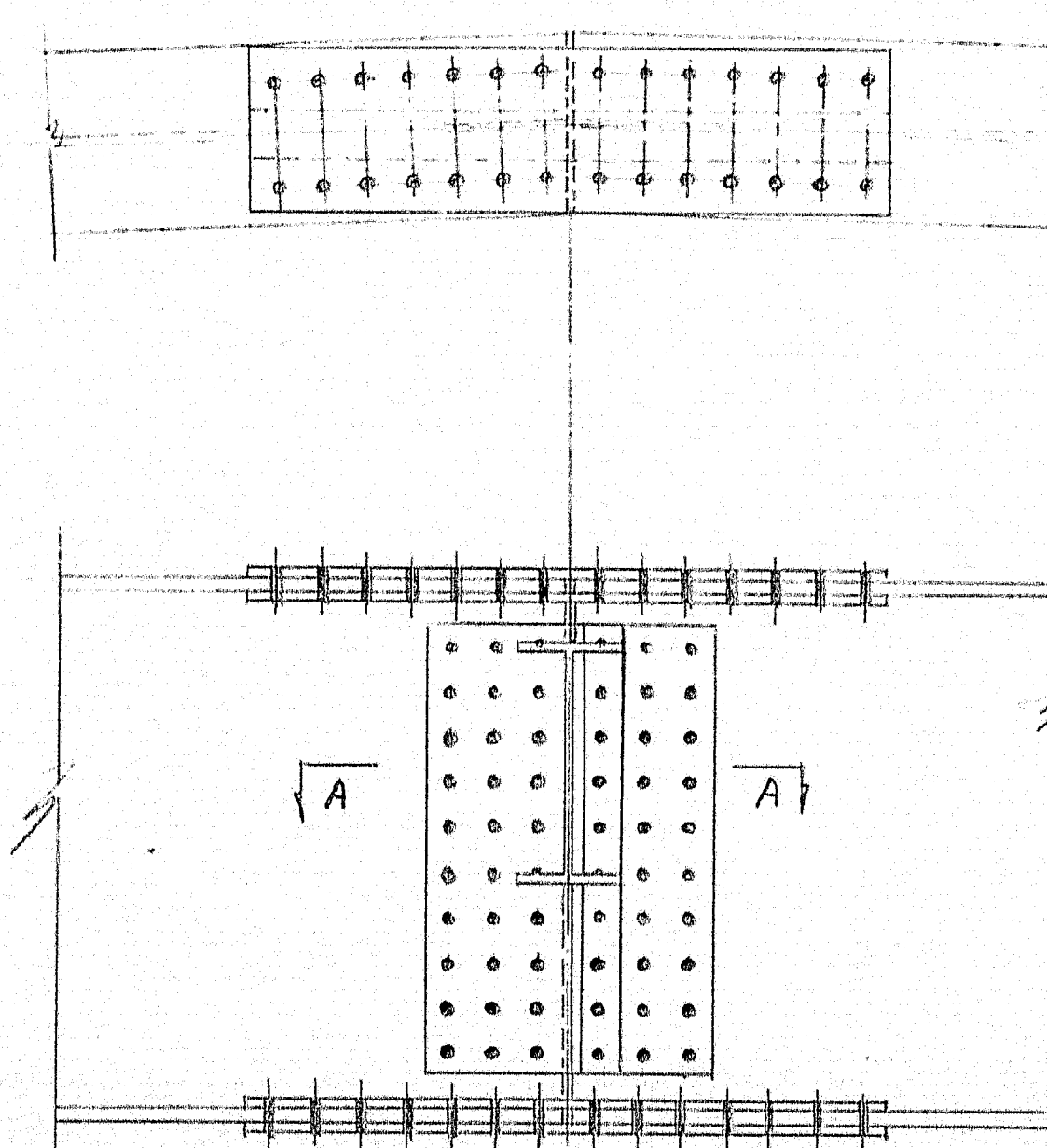


SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS: WELD & H.S. BOLT
HOLES: 1 1/2" ϕ
PAINT: STATE OF MAINE SPEC'S.

STRINGERS - M.C.R.R. - NORTHBOUND

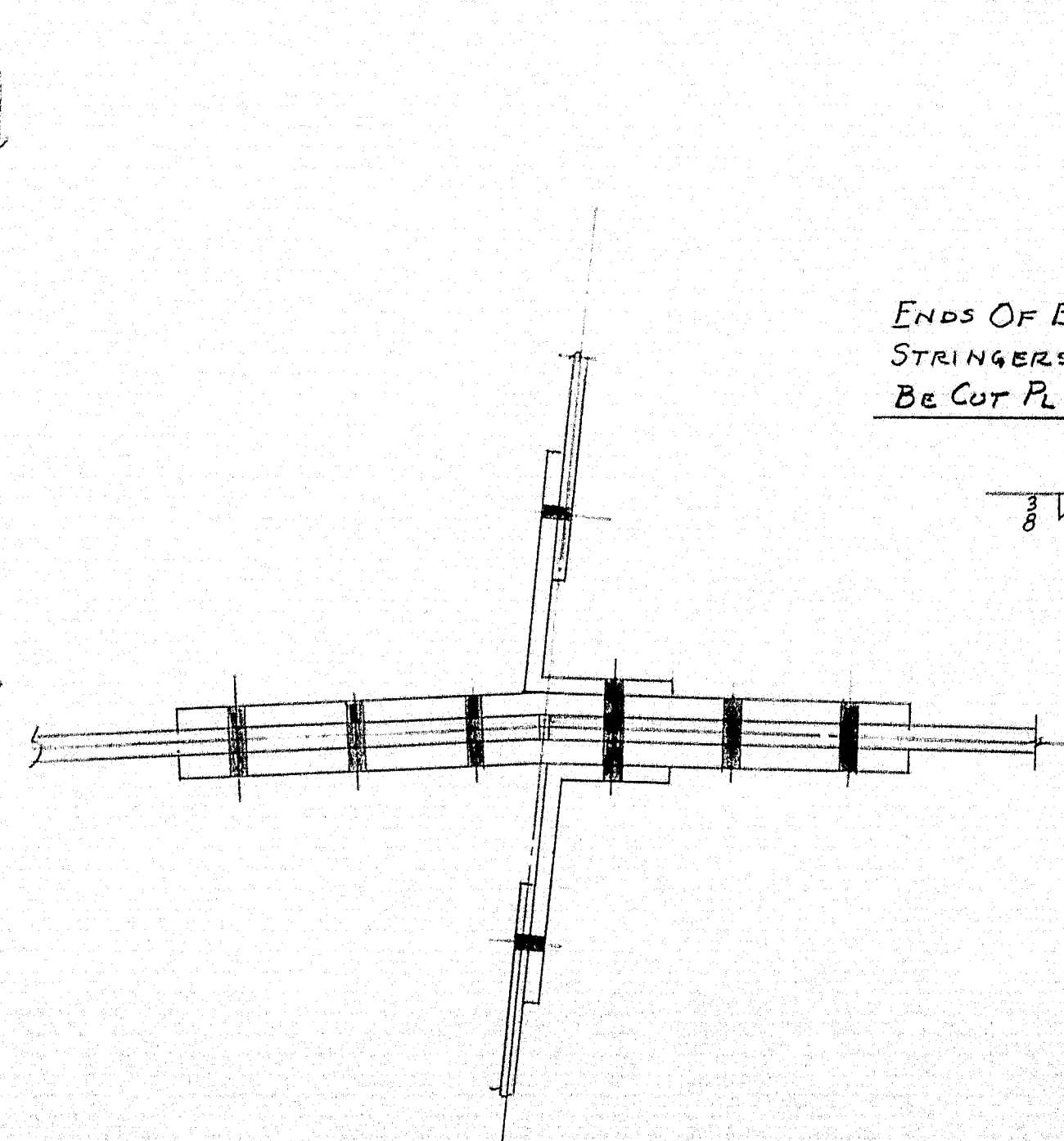


FRAMING PLAN

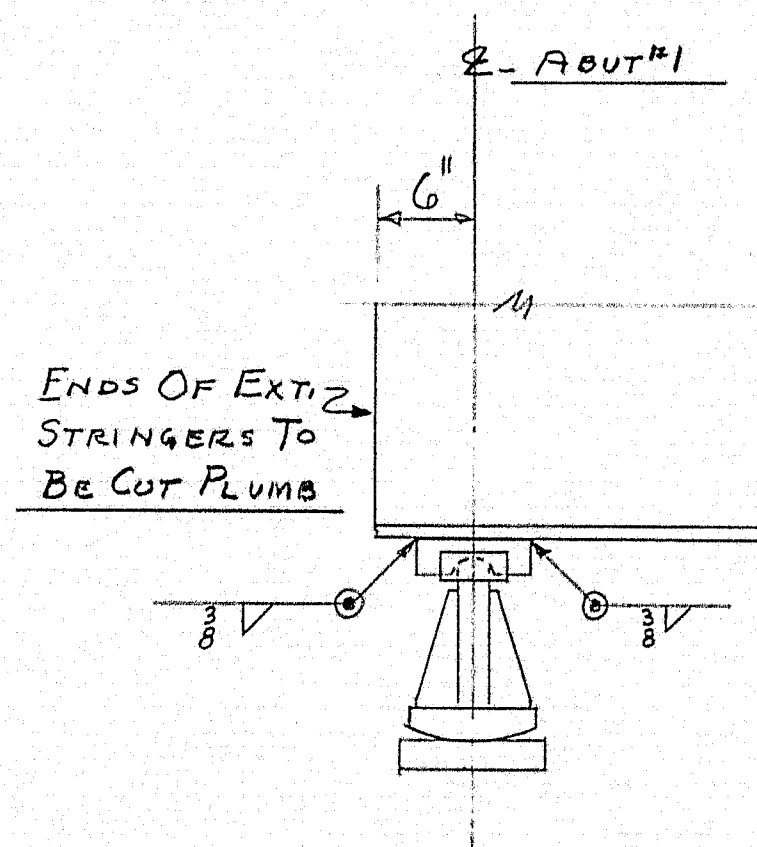


SPlice DETAIL

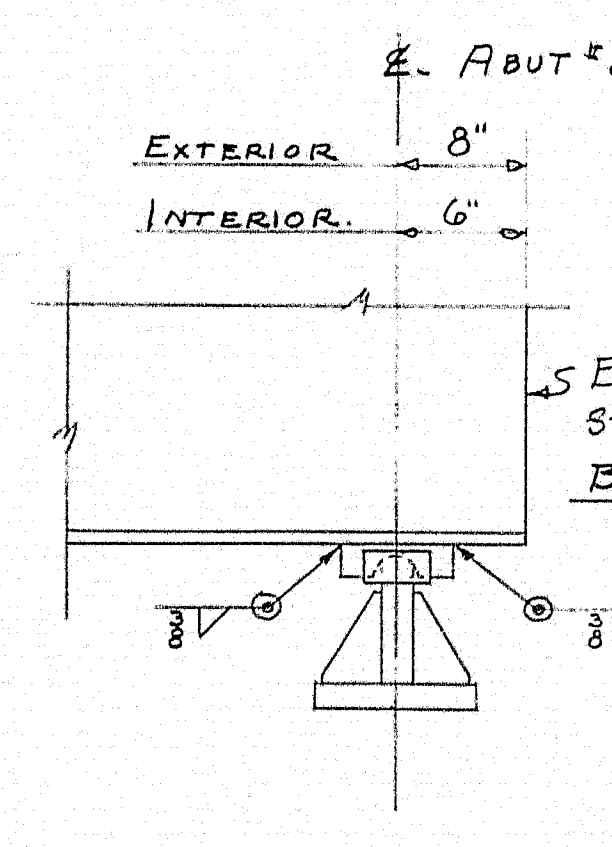
BOLTS FOR SPLICES SHALL BE ASTM A325 - 3/4" HEX. HEAVY HEAD STRUCT. BOLTS WITH HEAVY SEMI-FINISHED HEX NUTS & ONE HARDENED ROUND WASHER



SECTION A-A



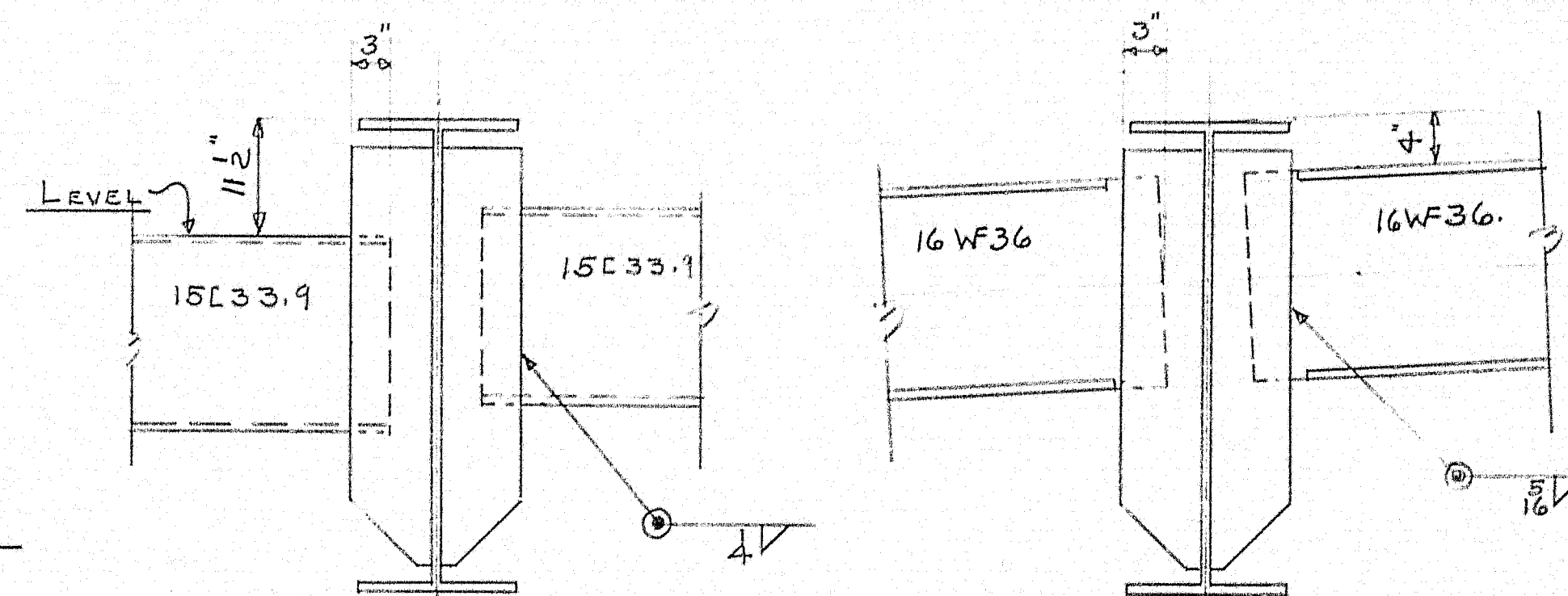
ENDS OF EXT. STRINGERS TO BE CUT PLUMB



END OF EXT. STRINGERS TO BE CUT PLUMB

BEARING DETAILS

SEE B63-188-S1 FOR LOCATION



DIAPHRAGM CONN'S.

SHOP CONNECTIONS:
FIELD CONNECTIONS:
HOLES:
PAINT:

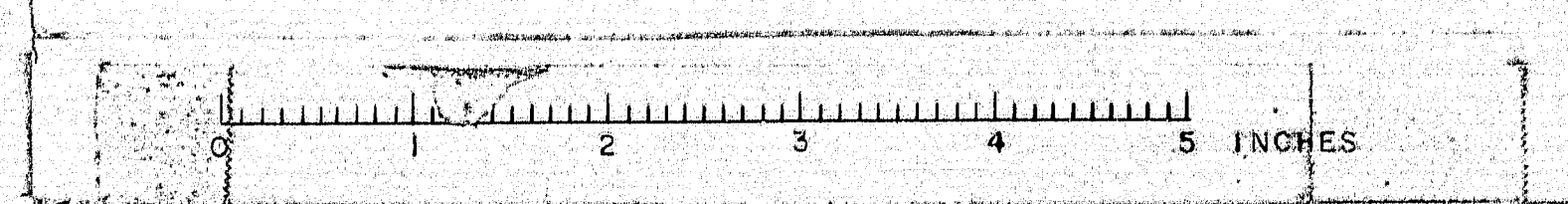
SPlice BOLT SCHEDULE	
SPlices #1 & #3	
56 - 3/4" x 3 3/4" H.S. BOLTS TOP & BOTT. FLG'e	
50 - 3/4" x 3 3/4" H.S. BOLTS AT WEB RS.	
10 - 3/4" x 4" H.S. BOLTS AT DIAPHRAGM RS.	
SPlice #2	
48 - 3/4" x 3 3/4" H.S. BOLTS TOP & BOTT. FLG'e	
60 - 3/4" x 3 3/4" H.S. BOLTS AT WEB RS.	

APP'D 12-10-63

FRAMING PLAN

PRINT ISSUE	
1	SHOP 12-20-63
2	SHC 12-20-63
3	CUT 12-20-63
2	F/A 12-6-63
2	F/A 11-4-63
DRAWN	8-21-63 D.C.
REVISION	
REVISION	
REVISION	

Bancroft & Martin Holdings, Mills Company	
Brewer, Maine	
INT. #95 OVER MCRR & RTE. 152	
PITTSFIELD MAINE	
CUSTOMER SEAWARD CONST. CO.	
DESIGNER STATE HIGHWAY COMM.	
ORDER VERBAL DWG. B63-188-E IS	



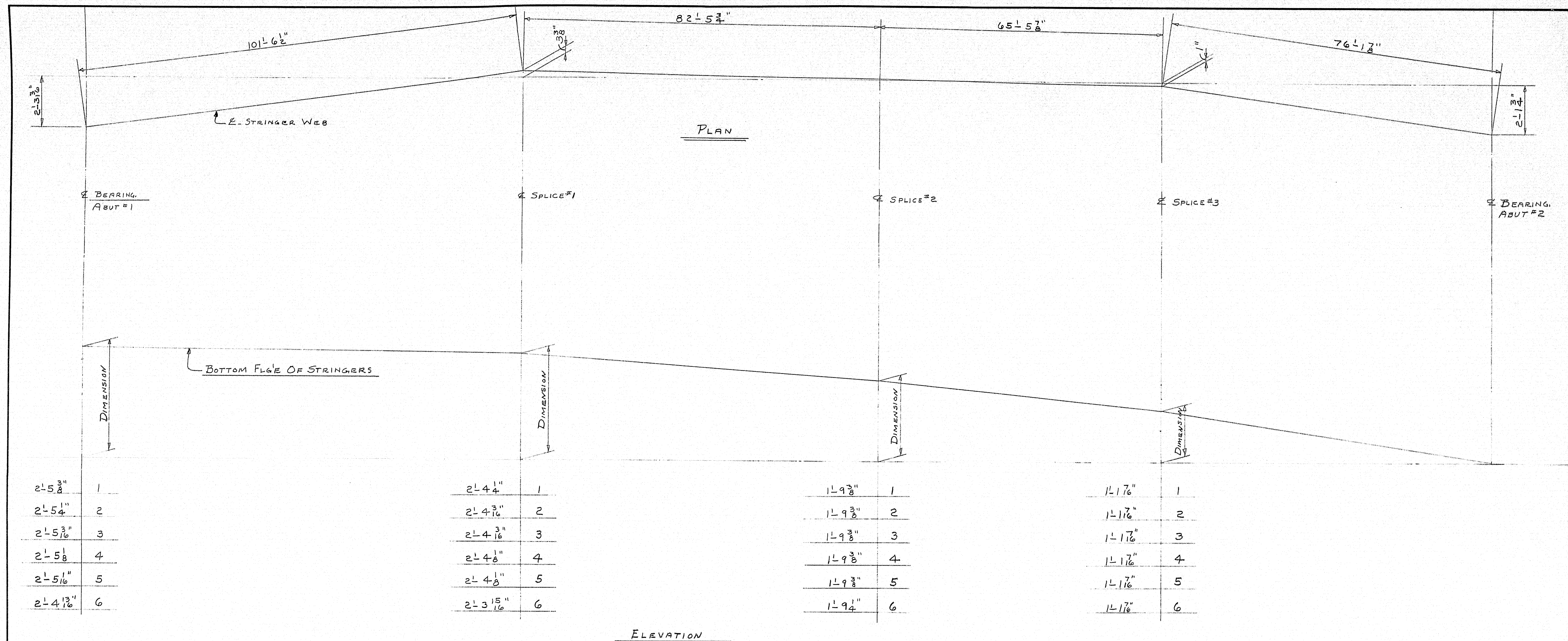
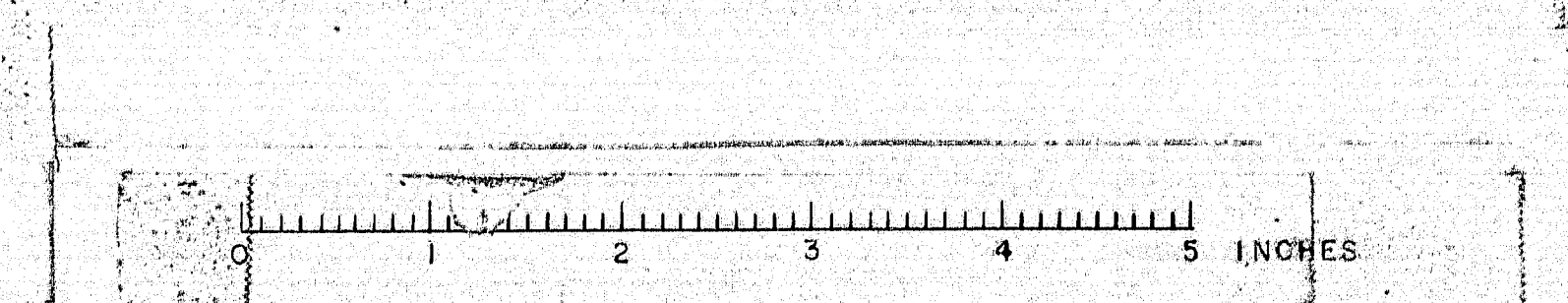


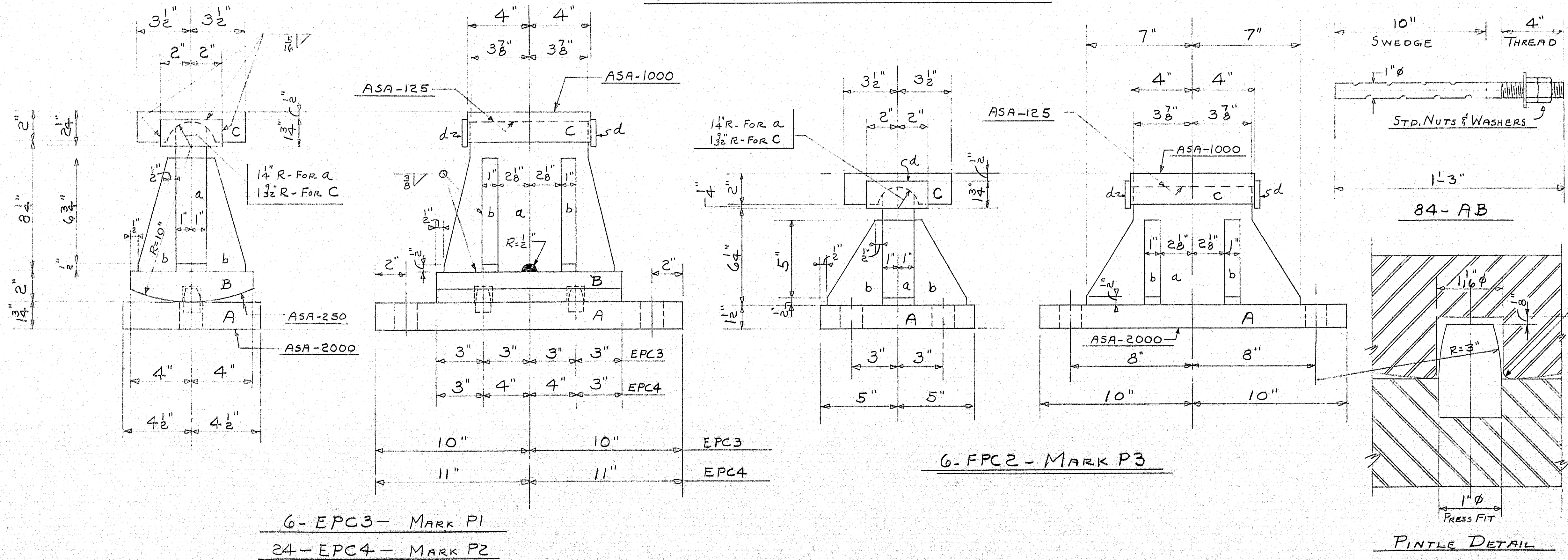
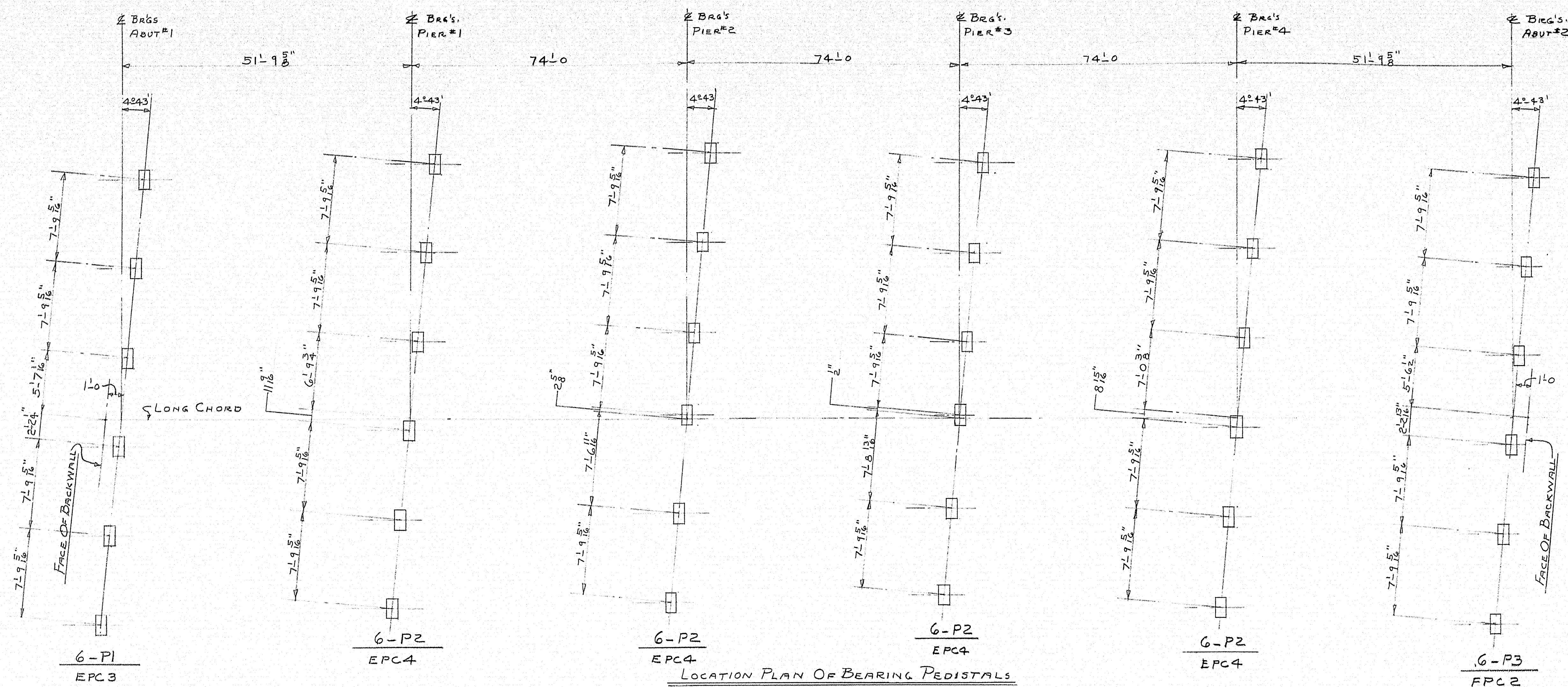
DIAGRAM OF STRINGER ELEVATIONS
 SET STRINGERS ON THESE ELEVATIONS TO
 POSITION & SUBPUNCH & REAM SPLICES.

SHOP CONNECTIONS:
 FIELD CONNECTIONS:
 HOLES:
 PAINT:

APP'D 11-7-63

DIAGRAM OF STRINGER ELEVATIONS		
PRINT ISSUE		
3	SHOP	12-20-63
2	SHC	12-20-63
3	COST	12-20-63
2	F/A	12-6-63
2	F/A	11-4-63
DRAWN 8-20-63 D.C.		
REVISION		
REVISION		
REVISION		
<i>Bancroft & Martin Rolling Mills Company</i> Brewer, Maine INT. 95 OVER MCRR & ROUTE 152 PITTSFIELD MAINE CUSTOMER SEAWARD CONST. CO. DESIGNER STATE HIGHWAY COMM. ORDER _____ DWG. B63-188-ERS		





SHIP		BILL OF MATERIAL			DWG. 363-188-S1	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
PIA	6		R 9 x 1 $\frac{3}{4}$	1	8	
P1B	6		R 8 x 2	1	0	
P1C	6		R 7 x 2	0	8	
P2A	24		R 9 x 1 $\frac{3}{4}$	1	10	
P2B	24		R 8 x 2	1	2	
P2C	24		R 7 x 2	0	8	
P3A	6		R 10 x 1 $\frac{1}{2}$	1	8	
P3C	6		R 7 x 2	0	8	
	6	P1a	R 11 x 2	0	9 $\frac{1}{2}$	
	24	P2a	R 13 x 2	0	9 $\frac{1}{2}$	
	6	P3a	R 14 x 2	0	7 $\frac{1}{2}$	
	24	P1b	R 2 $\frac{1}{2}$ x 1	0	7 $\frac{1}{2}$	
	96	P2b	D _o	0	7 $\frac{1}{2}$	
	24	P3b	R 3 $\frac{1}{2}$ x 1	0	5 $\frac{1}{2}$	
	72	d	R 1 $\frac{3}{4}$ x 3 $\frac{3}{8}$	0	4	
	60		1" ϕ R _{oD}	0	1 $\frac{1}{2}$	PINTLE
AB	84		1" ϕ R _{oD}	1	3	
	84		1" WASHERS			
	168		1" NUTS			
P4	6		9 x 8	1	8	} FABLOPAD 5A4-7
P5	24		9 x 8	1	10	
P6	6		10 x 8	1	8	

SHOP CONNECTIONS: WELD
FIELD CONNECTIONS: WELD
HOLES: $1\frac{3}{8}$ " ϕ
PAINT: STATE OF ME, SPECS.

APP'D AS NOTED 11-7-63

BEARING PEDISTALS SOUTHBOUND

PRINT ISSUE

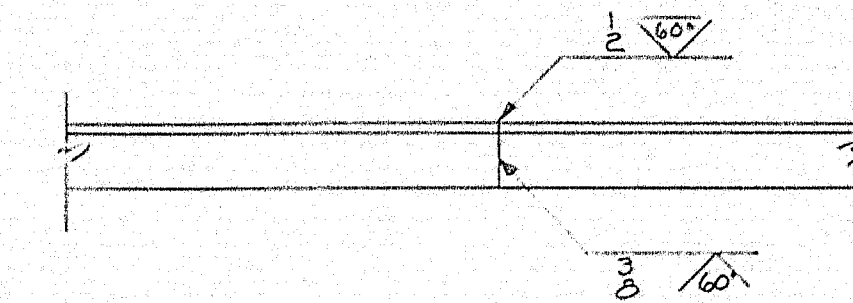
Bancroft & Martin Inc.
Brewer, Maine

2	SHC	12-20-63	INT. 95 OVER MCRR & ROUTE 152
3	CUST	12-20-63	PITTSFIELD MAINE
PER 12-20-63			

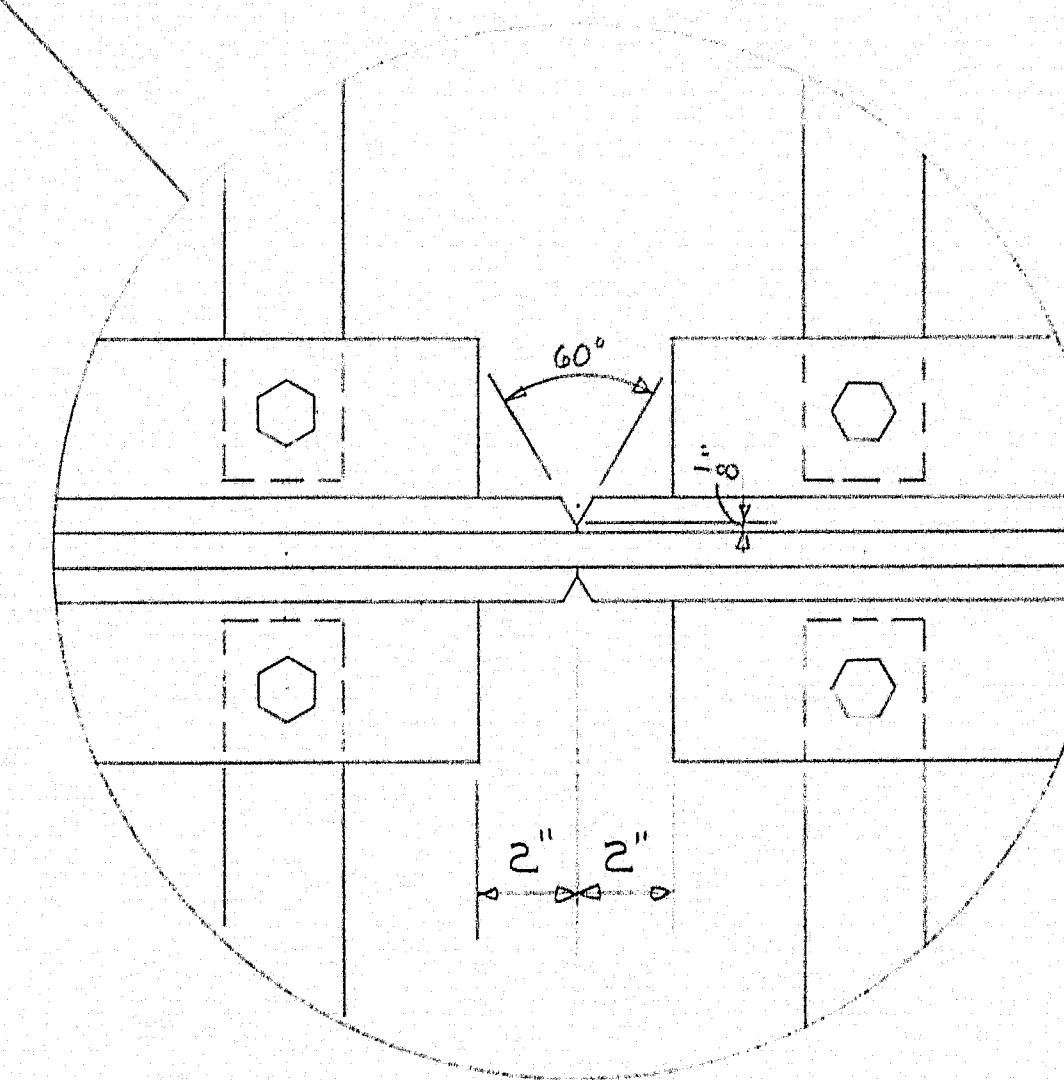
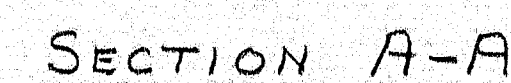
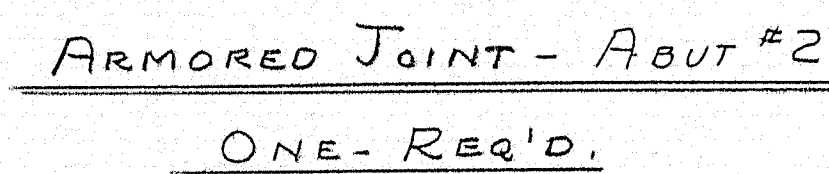
2	F/A	11-4-63	CUSTOMER <u>SEAWARD CONST. Co.</u>
DESIGN	8-15-63	D.C.	DESIGNER <u>STATE HIGHWAY COMM.</u>
REVISION			

REVISION			
REVISION			
REVISION			

ORDER <u>VERBAL</u>	DWG. <u>B63-188-SJS</u> SEGMENT (3)
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100



SHOP CONNECTIONS: 3 H.S. BOLTS
FIELD CONNECTIONS: WELD
HOLES: 1 1/2" Ø UNLESS NOTED
PAINT: STATE OF MAINE SPEC'S.

ARMORED JOINT ABUT^{#2} SOUTHBOUND

PRINT ISSUE			<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>	
2	S.H.C.	12-20-63	INT # 95 OVER MCRR & RTE, 152 PITTSFIELD MAINE CUSTOMER <u>SEAWARD CONST. Co.</u> DESIGNER <u>STATE HIGHWAY COMM.</u>	
3	CUST	12-20-63		
2	F/A	11-4-63		
5	SHOP	10-30-63		
DRAWN	8-22-63	D.C.	ORDER <u>VERBAL</u> DWG. <u>B63-128-535</u> 12-24-63	
REVISION				
REVISION				
REVISION				

